



**CENTRAL TEXAS
Regional Mobility Authority**

Regular Meeting of the Board of Directors

9:00 a.m
Wednesday, July 30, 2014

Lowell H. Lebermann, Jr., Board Room
3300 N. IH-35, Suite 300
Austin, Texas 78705

AGENDA

No action on the following:

1. Welcome and opening remarks by the Chairman and members of the Board of Directors.
2. Opportunity for public comment – See **Notes** at the end of this agenda.

Consent Agenda

The Consent Agenda includes routine or recurring items for Board action with a single vote. The Chairman or a Board Member may defer any Consent Agenda item for a separate consideration under the Regular Agenda.

3. Authorize a procurement for marketing services for the MoPac Express Lanes information campaign.
4. Authorize a procurement for services related to traffic signals, signage, and lighting on Mobility Authority roadways.

Regular Items

Items for the Board to discuss, consider, and take appropriate action.

5. Approve the minutes for the June 25, 2014, Regular Board Meeting.
6. Approve the unaudited financial statements for June 2014.
7. Approve an amendment to the contract with CDM Smith Inc. for traffic and revenue studies on Mobility Authority toll projects.

8. Authorize the Executive Director to execute an amendment to the advance funding agreement with the Texas Department of Transportation for the Bergstrom Expressway Project.
9. Authorize the Executive Director to execute an advance funding agreement with the Texas Department of Transportation for the Oak Hill Parkway Project.
10. Approve a work authorization with Atkins North America, Inc., to provide general engineering consultant services for the SH 71 Express Project.
11. Approve a work authorization with Telvent USA, LLC, for toll system design and integration services for the SH 71 Express Project.
12. Approve a resolution supporting the proposed amendment to the Texas Constitution providing for increased state funding for roads, highways and bridges.

Briefings and Reports

Items for briefing and discussion, but no action to be taken by the Board.

13. Quarterly briefing on the MoPac Improvement Project.
14. Quarterly briefing on the following projects:
 - A. Maha Loop/Elroy Road
 - B. 183/183A Intersection
 - C. SH 71 Express
 - D. Bergstrom Expressway
 - E. Oak Hill Parkway
 - F. MoPac South
 - G. MoPac Intersections
 - H. 183 North
 - I. SH 45 SW
15. Presentation on traffic modeling for the SH 45 SW project.
16. Executive Director's report.
 - A. Interlocal agreement with City of Leander and development project adjacent to the 183/183A Intersection.

- B. City of Austin City Council candidate presentation.

Executive Session

Under Chapter 551 of the Texas Government Code, the Board may recess into a closed meeting (an executive session) to deliberate any item on this agenda if the Chairman announces the item will be deliberated in executive session and identifies the section or sections of Chapter 551 that authorize meeting in executive session. A final action, decision, or vote on a matter deliberated in executive session will be made only after the Board reconvenes in an open meeting.

The Board may deliberate the following items in executive session if announced by the Chairman:

17. Discuss acquisition of one or more parcels or interests in real property needed for the Manor Expressway Project as authorized by §551.072 (Deliberation Regarding Real Property), and related legal issues as authorized by §551.071 (Consultation With Attorney).
18. Discuss legal issues relating to procurement and financing of Mobility Authority transportation projects, as authorized by §551.071 (Consultation With Attorney).
19. Discuss legal issues related to claims by or against the Mobility Authority, pending or contemplated litigation and any related settlement offers; or other matters as authorized by §551.071 (Consultation With Attorney).

Reconvene in Open Session.

20. Authorize negotiation and execution of an agreement with William K. Reagan and Reagan National Advertising of Austin, Inc. to resolve outstanding claims relating to acquisition of property interests in Parcel 17, a 0.899 acre parcel of real estate located at 9207 US Hwy 290E in Travis County, necessary for construction of the Manor Expressway (290 Toll) project.
21. Adjourn Meeting.

Notes

Opportunity for Public Comment. At the beginning and at the end of the meeting, the Board provides a period of up to one hour for public comment on any matter subject to the Mobility Authority's jurisdiction. Each speaker is allowed a maximum of three minutes. A person who wishes to address the Board should sign the speaker registration sheet before the beginning of the public comment period. If a speaker's topic is not listed on this agenda, the Board may not deliberate the speaker's topic or question the speaker during the open comment period, but may direct staff to investigate the matter or propose that an item be placed on a subsequent agenda for deliberation and possible action by the Board. The Board may not deliberate or act on an item that is not listed on this agenda.

*Mobility Authority Board Meeting Agenda
Wednesday, July 30, 2014*

Public Comment on Agenda Items. A member of the public may offer comments on a specific agenda item in open session if he or she signs the speaker registration sheet for that item before the Board takes up consideration of the item. The Chairman may limit the amount of time allowed for each speaker. Public comment unrelated to a specific agenda item must be offered during the open comment period.

Meeting Procedures. The order and numbering of agenda items is for ease of reference only. After the meeting is convened, the Chairman may rearrange the order in which agenda items are considered, and the Board may consider items on the agenda in any order or at any time during the meeting.

Persons with disabilities. If you plan to attend this meeting and may need auxiliary aids or services, such as an interpreter for those who are deaf or hearing impaired, or if you are a reader of large print or Braille, please contact Jennifer Guernica at (512) 996-9778 at least two days before the meeting so that appropriate arrangements can be made.

Español. Si desea recibir asistencia gratuita para traducir esta información, llame al (512) 996-9778.



CENTRAL TEXAS
Regional Mobility Authority

AGENDA ITEM #1 SUMMARY

Welcome, Opening Remarks and Board Member Comments.

Welcome, Opening Remarks and Board Member Comments

Board Action Required: No



**CENTRAL TEXAS
Regional Mobility Authority**

AGENDA ITEM #2 SUMMARY

Open Comment Period for Public Comment.
Public Comment on Agenda Items.

Open Comment Period for Public Comment - At the beginning of the meeting, the Board provides a period of up to one hour for public comment on any matter subject to CTRMA's jurisdiction. Each speaker is allowed a maximum of three minutes. A person who wishes to address the Board should sign the speaker registration sheet before the beginning of the open comment period. If the speaker's topic is not listed on this agenda, the Board may not deliberate the topic or question the speaker during the open comment period, but may direct staff to investigate the subject further or propose that an item be placed on a subsequent agenda for deliberation and possible action by the Board. The Board may not act on an item that is not listed on this agenda.

Public Comment on Agenda Items - A member of the public may offer comments on a specific agenda item in open session if he or she signs the speaker registration sheet for that item before the Board's consideration of the item. The Chairman may limit the amount of time allowed for each speaker. Public comment unrelated to a specific agenda item must be offered during the open comment period.

Board Action: No



**CENTRAL TEXAS
Regional Mobility Authority**

AGENDA ITEM #3 SUMMARY

Authorize a procurement for marketing services for the MoPac Express Lanes information campaign.

Strategic Plan Relevance: Regional Mobility, Innovation

Department: Community Relations

Associated Costs: \$950,000 (estimated)

Funding Source: MoPac Improvement Project Capital Budget

Board Action Required: Authorization to Procure Consultant

Description of Matter:

To facilitate a smooth and successful deployment of the MoPac Express Lanes, staff is requesting authority to procure an Advertising and Marketing firm to develop and implement an innovative informational publicity campaign.

Reference documentation: See attached memo
Draft Resolution

Contact for further information: Steve Pustelnyk, Director of Community Relations

Memo



CENTRAL TEXAS
Regional Mobility Authority

To: Board Members
From: Steve Pustelnyk
Date: July 16, 2014
Re: MoPac Advertising and Marketing Firm

The MoPac Improvement Project will result in the deployment of the first tolled Express Lanes with variable tolls in Central Texas. Given the unique characteristics of the tolled Express Lanes, a comprehensive informational and publicity campaign will be undertaken to facilitate a smooth deployment of the new lanes.

As part of the capital budget for the MoPac Improvement Project, staff allocated funding for the campaign. The vast majority of the funding would be used to pay for advertising placed in various mediums such as social media, online, print, radio, television, and direct mail. Staff is requesting approval to hire an Advertising and Marketing Firm to develop and implement the informational and publicity campaign.

The selected firm would be responsible for developing a campaign strategy intended to reach key audiences and ensure they understand how the new Express Lanes work. The firm would be tasked with developing the creative elements for the campaign and would handle production of the various elements such as newspaper ads and radio spots. Finally, the firm would negotiate with various media companies to maximize the reach of the informational and publicity campaign to ensure the goals of the campaign are achieved.

**GENERAL MEETING OF THE BOARD OF DIRECTORS
OF THE
CENTRAL TEXAS REGIONAL MOBILITY AUTHORITY**

RESOLUTION NO. 14-__

**AUTHORIZING A PROCUREMENT FOR MARKETING SERVICES FOR THE
MOPAC EXPRESS LANES INFORMATION CAMPAIGN**

WHEREAS, operation of the completed MoPac Improvement Project will bring the first tolled express lanes using variable tolls to Central Texas; and

WHEREAS, because the operating characteristics of an express lane with a variable toll are different than existing toll roads in Central Texas, the Mobility Authority needs to provide information to drivers who may use those express lanes to facilitate a smooth deployment of the express lanes when the MoPac Improvement Project opens; and

WHEREAS, the Executive Director recommends initiating a procurement to solicit marketing services for the MoPac Express Lanes Information Campaign.

NOW THEREFORE, BE IT RESOLVED, that the Executive Director is authorized and directed to procure marketing services for the MoPac Express Lanes Information Campaign in accordance with the procurement policies established by Chapter 4 of the Mobility Authority Policy Code.

Adopted by the Board of Directors of the Central Texas Regional Mobility Authority on the 30th day of July, 2014.

Submitted and reviewed by:

Approved:

Andrew Martin
General Counsel for the Central
Texas Regional Mobility Authority

Ray A. Wilkerson
Chairman, Board of Directors
Resolution Number: 14-__
Date Passed: 07/30/14



**CENTRAL TEXAS
Regional Mobility Authority**

AGENDA ITEM #4 SUMMARY

Authorize a procurement for services related to traffic signals, signage, and lighting on Mobility Authority roadways.

Strategic Plan Relevance: Regional Mobility

Department: Engineering

Associated Costs: TBD

Funding Source: General Funds

Board Action Required: Yes

Description of Matter:

The current contract expires on November 30, 2014, and provides on-call maintenance and miscellaneous repair services only for the 183A Toll Road. A new contract will provide on-call maintenance and miscellaneous repair services for all Mobility Authority roadways, current and future, for the term of the contract.

A Request for Bids and associated documents will be prepared for the project in anticipation of advertising. Recommendations based on reviews of the bids when they are received will be presented by staff to the Board of Directors for final approval of the award of contract.

Reference documentation: Draft Resolution

Contact for further information: Wesley M. Burford, P.E. Director of Engineering

**GENERAL MEETING OF THE BOARD OF DIRECTORS
OF THE
CENTRAL TEXAS REGIONAL MOBILITY AUTHORITY**

RESOLUTION NO. 14-___

**AUTHORIZING A PROCUREMENT FOR SERVICES RELATED TO
TRAFFIC SIGNALS, SIGNAGE, AND LIGHTING ON MOBILITY
AUTHORITY ROADWAYS.**

WHEREAS, to continue needed services to maintain and repair traffic signals, signage, and lighting as well as miscellaneous related services on Mobility Authority roadways, the Mobility Authority needs to procure a new contract for such services; and

WHEREAS, the Executive Director recommends that the Mobility Authority procure a contract to provide these services for Mobility Authority road corridors.

NOW THEREFORE, BE IT RESOLVED that the Executive Director is authorized and directed to procure a contract for services to maintain and repair traffic signals, signage, and lighting as well as miscellaneous related services on Mobility Authority roadways in accordance with the procurement policies established by Chapter 4 of the Mobility Authority Policy Code.

Adopted by the Board of Directors of the Central Texas Regional Mobility Authority on the 30th day of July, 2014.

Submitted and reviewed by:

Approved:

Andrew Martin
General Counsel for the Central
Texas Regional Mobility Authority

Ray A. Wilkerson
Chairman, Board of Directors
Resolution Number 14-____
Date Passed 07/30/14



**CENTRAL TEXAS
Regional Mobility Authority**

AGENDA ITEM #5 SUMMARY

Approve the minutes for the June 25, 2014,
Regular Board Meeting.

Department: Law
Funding Source: None
Board Action Required: Yes (by Motion)

Description of Matter:

Draft Minutes for the June 25, 2014, Regular Board Meeting

Reference documentation: Draft Minutes, June 25, 2014, Regular Board Meeting

Contact for further information: Andrew Martin, General Counsel

MINUTES

Regular Meeting of the Board of Directors of the CENTRAL TEXAS REGIONAL MOBILITY AUTHORITY

Wednesday, June 25, 2014
9:00 A.M.

The meeting was held in the Mobility Authority's Lowell H. Lebermann, Jr., Board Room at 3300 N. Interstate 35, #300, Austin, Texas 78705-1849. Notice of the meeting was posted June 20, 2014 at the respective County Courthouses of Williamson and Travis Counties; online on the website of the Secretary of State; online on the website of the Mobility Authority; and in the Mobility Authority's office lobby at 3300 N. Interstate 35, #300, Austin, Texas 78705-1849.

1. Welcome and Opening Remarks by Chairman Ray Wilkerson.

Chairman Ray Wilkerson called the meeting to order at 9:04 a.m. and called the roll. Directors present at the time the meeting was called to order were Mr. Jim Mills, Ms. Nikelle Meade, Mr. David Singleton, and Mr. David Armbrust. Mr. Charles Heimsath joined the dais after item 3. Mr. Bob Bennett was not present for the meeting.

2. Open Comment Period.

No public comments were given during the open comment period.

3. Introduction and presentation by PMB Helin Donovan, LLP, auditors for the Mobility Authority Fiscal Year 2014 audit.

Don McFee with PMB Helin Donovan provided a brief presentation on the Fiscal Year 2014 audit for the Mobility Authority.

4. Approve the minutes for the May 21, 2014, Regular Board Meeting.

Chairman Ray Wilkerson presented for Board consideration the minutes for the May 21, 2014, Regular Board Meeting. Mr. David Singleton moved to approve the minutes as drafted, and Mr. Charles Heimsath seconded the motion. The motion carried unanimously 6-0, and the minutes were approved as drafted.

5. Approve an amendment to the contract with Crosswind Communications, LLC, for communication and marketing services.

Mr. Mario Espinoza presented this item. Due to the increased requirement for services related to all our projects under development and construction, staff is recommending a supplement of \$250,000 for each of the contract extensions. This will increase the total not to exceed amount for each contract to \$650,000 for the contracts ending July 2014 and July 2015.

Ms. Nikelle Meade moved for approval, and Mr. David Armbrust seconded the motion. The motion carried unanimously, 6-0, and the resolution was approved as drafted.

6. Approve a personal services contract with Neal Spelce to provide community outreach services for the MoPac Improvement Project.

Mr. Mario Espinoza presented this item. As part of its extensive community outreach efforts related to the construction of the MoPac Improvement Project, the Mobility Authority administratively contracted with Neal Spelce on a five month pilot basis to assist in communication and outreach efforts. With the pilot contract near expiration, staff is recommending a 12 month contract with Mr. Spelce for communication and community outreach services for a total contract amount not to exceed \$60,000.

Mr. Charles Heimsath moved for approval, and Ms. Nikelle Meade seconded the motion. The motion carried unanimously, 6-0, and the resolution was approved as drafted.

7. Authorize the Executive Director to negotiate and execute an agreement with Metropia, Inc., for transportation management software, consistent with funding approval by the Federal Highway Administration.

Mr. Andy Martin presented this item. Funding to provide the Metropia software as an “experimental project” is currently under consideration by the Federal Highway Administration. The draft resolution authorizes the Executive Director to negotiate and execute a contract between the Authority and Metropia that is consistent with terms and conditions of FHWA funding approval.

Ms. Nikelle Meade moved for approval, and Mr. Charles Heimsath seconded the motion. The motion carried unanimously, 6-0, and the resolution was approved as drafted.

8. Approve a change order with Central Texas Mobility Constructors LLC for the Manor Expressway Phase II Project.

Mr. Wes Burford presented this item. Staff requests Board approval of a change order with Central Texas Mobility Constructors, LLC (“CTMC”) on the Manor Expressway Phase II Project to compensate CTMC for installing stone and concrete riprap as well as landscape curb in and around the US 183 interchange. These changes were made to reduce future maintenance costs.

Mr. David Armbrust moved for approval, and Mr. David Singleton seconded the motion. The motion carried unanimously, 6-0, and the resolution was approved as drafted.

9. Award a contract for milling and overlay work on 183A frontage roads.

Mr. Wes Burford presented this item. Staff recommends that the contract be awarded to the lowest responsive bidder, Ramming Paving Company.

Mr. David Armbrust moved for approval, and Mr. Jim Mills seconded the motion. The motion carried unanimously, 6-0, and the resolution was approved as drafted.

10. Authorize a procurement of a construction contract for improvements to the US 183/183A intersection.

Mr. Wes Burford presented this item. The US 183/183A Intersection Project is designed to improve safety and driver expectation issues at the US 183 and 183A intersection in Leander, Texas, which also includes the adjacent intersection of US 183 and CR 276. In addition to improving safety for the traveling public, the proposed improvements will provide improved access to the intersection.

Mr. David Singleton moved for approval, and Ms. Nikelle Meade seconded the motion. The motion carried unanimously, 6-0, and the resolution was approved as drafted.

11. Accept the Risk Management Audit performed by The Hidler Group, Inc.

Mr. Bill Chapman presented this item.

Mr. David Singleton moved for approval, and Ms. Nikelle Meade seconded the motion. The motion carried unanimously, 6-0, and the resolution was approved as drafted.

12. Approve the Investment Policy (Article 1, Chapter 2 of the Policy Code) for Fiscal Year 2015.

Mr. Bill Chapman presented that there were no changes to the Investment Policy.

Mr. Charles Heimsath moved for approval, and Mr. Jim Mills seconded the motion. The motion carried unanimously, 6-0, and the resolution was approved as drafted.

13. Approve the financial institutions and brokers authorized to provide investment services to the Mobility Authority for Fiscal Year 2015.

Mr. Bill Chapman presented that there were no additions to the financial institutions and brokers authorized to provide investment services.

Mr. Jim Mills moved for approval, and Ms. Nikelle Meade seconded the motion. The motion carried unanimously, 6-0, and the resolution was approved as drafted.

14. Approve the 2014 Mobility Authority Strategic Plan.

Ms. Cindy Demers presented this item and provided the changes made to the Mobility Authority Strategic Plan.

Mr. Charles Heimsath moved for approval, and Ms. Nikelle Meade seconded the motion. The motion carried unanimously, 6-0, and the resolution was approved as drafted.

15. Approve the budget for Fiscal Year 2015.

Mr. Bill Chapman and Ms. Cindy Demers presented this item and provided an explanation of the budget for Fiscal Year 2015.

Ms. Nikelle Meade moved for approval, and Mr. Charles Heimsath seconded the motion. The motion carried unanimously, 6-0, and the resolution was approved as drafted.

16. Approve the financial statements for May 2014.

Mr. Bill Chapman presented this item. There was nothing unusual to report on the May 2014 financial statements.

Mr. Charles Heimsath moved for approval, and Ms. Nikelle Meade seconded the motion. The motion carried unanimously, 6-0, and the resolution was approved as drafted.

Briefing and discussion on the following:

17. Executive Director's report.

Mr. Mike Heiligenstein presented this item and provided a brief update on positive traffic trends, the communications summit held with Mobility Authority staff and contractors, as well as a discussion on contracts with local firms. Melissa Hurst provided an update on public comments from Open Houses for the 183 North Project, SH 45 SW Project, and the MoPac South Project. Carlos Lopez of HNTB provided an update on the I-35 Stakeholder Working Group.

11. Adjourn Meeting.

Chairman Ray Wilkerson declared the meeting adjourned at 11:20 a.m. with unanimous consent.



**CENTRAL TEXAS
Regional Mobility Authority**

AGENDA ITEM #6 SUMMARY

Accept the unaudited financial statements for June 2014.

Department: Finance

Funding Source: None

Board Action Required: Yes

Description of Matter:

Presentation and acceptance of the unaudited monthly financial statements for June 2014

Reference documentation: Draft Resolution

Unaudited Draft Financial Statements for June 2014

Contact for further information: Bill Chapman, Chief Financial Officer

Cindy Demers, Controller

**GENERAL MEETING OF THE BOARD OF DIRECTORS
OF THE
CENTRAL TEXAS REGIONAL MOBILITY AUTHORITY**

RESOLUTION NO. 14-___

ACCEPT THE UNAUDITED FINANCIAL STATEMENTS FOR JUNE 2014.

WHEREAS, the Central Texas Regional Mobility Authority (“Mobility Authority”) is empowered to procure such goods and services as it deems necessary to assist with its operations and to study and develop potential transportation projects, and is responsible to insure accurate financial records are maintained using sound and acceptable financial practices; and

WHEREAS, close scrutiny of the Mobility Authority’s expenditures for goods and services, including those related to project development, as well as close scrutiny of the Mobility Authority’s financial condition and records is the responsibility of the Board and its designees through procedures the Board may implement from time to time; and

WHEREAS, the Board has adopted policies and procedures intended to provide strong fiscal oversight and which authorize the Executive Director, working with the Mobility Authority’s Chief Financial Officer, to review invoices, approve disbursements, and prepare and maintain accurate financial records and reports; and

WHEREAS, the Executive Director, working with the Chief Financial Officer, has reviewed and authorized the disbursements necessary for the month of June 2014, and has caused Financial Statements to be prepared and attached to this resolution as Attachments A.

NOW THEREFORE, BE IT RESOLVED, that the Board of Directors accepts the Financial Statements for June 2014, attached as Attachments A.

Adopted by the Board of Directors of the Central Texas Regional Mobility Authority on the 30th day of July, 2014.

Submitted and reviewed by:

Approved:

Andrew Martin
General Counsel for the Central
Texas Regional Mobility Authority

Ray A. Wilkerson
Chairman, Board of Directors
Resolution Number: 14-___
Date Passed: 7/30/2014

Attachment A

Financial Statements for June 2014

**Central Texas Regional Mobility Authority
Balance Sheet**

As of

June 30, 2014

June 30, 2013

Assets

Current Assets

Cash in Regions Operating Account	539,024		773,079	
Cash In TexSTAR	60,036		345,711	
Regions Payroll Account	37,104		21,543	
Restricted cash/cash equivalents				
Fidelity Government MMA	208,964,784		125,217,295	
Restricted Cash-TexStar	11,709,070		23,657,818	
Overpayment accounts	42,779		32,994	
Total Cash and Cash Equivalents		221,352,797		150,048,440
Accounts Receivable	332,499		235,164	
Due From Other Funds	520,815		214,060	
Due From TTA	365,208		1,308,875	
Due From NTTA	244,783		90,741	
Due From HCTRA	245,324		229,783	
Due From TxDOT	431,072		25,576,199	
Due From Federal Government	0		0	
Interest Receivable	114,013		216,923	
Total Receivables		2,253,715		27,871,745
Short Term Investments				
Short Term Investments		53,934,074		96,537,752
Other Current Assets				
Prepaid Insurance		22,842		26,034
Total Current Assets		277,603,831		274,508,814
Construction Work In Process		67,905,410		301,319,123

Fixed Assets

Computers(net)	81,525		20,340	
Computer Software(net)	1,162,799		0	
Furniture and Fixtures(net)	0		0	
Equipment(net)	10,871		21,134	
Autos and Trucks(net)	7,473		14,371	
Buildings and Toll Facilities(net)	5,832,776		6,009,891	
Highways and Bridges(net)	621,281,670		323,712,356	
Communication Equipment(net)	572,003		768,118	
Toll Equipment(net)	22,619,765		12,188,022	
Signs(net)	11,763,747		8,791,517	
Land Improvements(net)	13,069,807		3,814,884	
Right of Way	85,152,004		46,458,304	
Leasehold Improvements	168,191		71,337	
Total Fixed Assets		761,722,633		401,870,274

Long Term Investments

Other Assets

Security Deposits	0		0	
Intangible Assets	15,032,168		15,032,417	
2005 Bond Insurance Costs	5,141,990		5,337,706	
Total Assets		1,127,406,033		998,068,335

Liabilities

Current Liabilities

Accounts Payable	6,942,025	11,663,153
Overpayments	44,349	34,260
Salaries Payable	76,939	76,786
Interest Payable	19,924,481	16,489,704
Due to other Funds	520,815	214,060
Deferred Compensation Payable	0	0
TCDRS Payable	45,535	34,934
Medical Reimbursement Payable	0	0
Due to other Entities	462,291	465,504
FICA Payable		(0)
FICA MED Payable		0
Federal Withholding Payable		0
Due to State of Texas	0	(0)
Total Current Liabilities	43,760,091	28,978,401

Long Term Liabilities

Accrued Vac & Sick Leave Paybl	189,089	189,089
Senior Lien Revenue Bonds 2005	0	0
Senior Lien Revenue Bonds 2010	108,127,115	104,661,359
Senior Lien Revenue Bonds 2011	307,980,943	307,258,700
Senior Refunding Bonds 2013	184,710,000	185,810,000
Sn Lien Rev Bnd Prem/Disc 2010	93,168	132,028
Sn Lien Rev Bnd Prem/Disc 2011	(3,577,246)	(3,718,622)
Sn Lien Rev Bnd Prem/Disc 2013	16,849,976	19,502,441
Subordinated Lien Bond 2011	70,000,000	70,000,000
Subordinated Refunding Bonds 2013	103,710,000	103,960,000
Sub Lien Bond 2011 Prem/Disc	(1,887,022)	(1,985,003)
Sub Lien Bond 2013 Prem/Disc	3,794,125	4,286,179
2011 Regions Draw Down Note	3,049,820	1,974,569
2013 American Bank Loan	5,300,000	5,300,000
Total Long Term Liabilities	798,339,967	797,370,740
Total Liabilities	842,100,057	826,349,141

Net Assets Section

Contributed Capital	21,597,060	18,334,846
Net Assets beginning	153,384,277	75,761,003
Current Year Operations	110,324,638	77,623,346
Total Net Assets	263,708,915	153,384,349
Total Liabilities and Net Assets	1,127,406,033	998,068,335

**Central Texas Regional Mobility Authority
Income Statement
All Operating Departments**

Account Name	Budget Amount FY 2014	Actual Year to Date 6/30/2014	Percent of Budget	Actual Prior Year to Date 6/30/2013
Revenue				
Operating Revenue				
Toll Revenue-TxTag-Manor	1,188,228	2,761,377	232.39%	1,027,475
Toll Revenue-TxTag-183A	29,507,860	24,519,329	83.09%	22,123,788
Toll Revenue-HCTRA-183A	884,163	1,236,171	139.81%	978,990
Toll Revenue-HCTRA Manor	173,689	444,162	255.72%	128,233
Toll Revenue-NTTA-183A	580,498	995,933	171.57%	660,239
Toll Revenue-NTTA-Manor	77,633	126,065	162.39%	36,256
Video Tolls 183A	4,243,980	6,173,588	145.47%	4,953,088
Video Tolls Manor Expressway	452,664	784,849	173.38%	273,066
Fee revenue 183A	1,661,750	2,378,799	143.15%	1,810,913
Fee revenue Manor Expressway	179,820	547,858	304.67%	167,109
Total Operating Revenue	38,950,285	39,968,131	102.61%	32,159,156
Other Revenue				
Interest Income	180,000	200,226	111.24%	230,171
Grant Revenue	1,236,000	112,225,641	9080%	92,205,336
Reimbursed Expenditures	-	-	0.00%	34,774
Misc Revenue	92,500	2,907,434	3143%	378,310
Unrealized Loss	-	-	0.00%	42,708
Total Other Revenue	1,508,500	115,333,301	7646%	92,891,299
Total Revenue	\$ 40,458,785	\$ 155,301,432	383.85%	\$ 125,050,455
Expenses				
Salaries and Wages				
Salary Expense-Regular	2,185,005	1,931,733	88.41%	1,833,681
Part Time Salary Expense	12,000	-	0.00%	480
Overtime Salary Expense	3,000	-	0.00%	-
Contractual Employees Expense	5,000	-	0.00%	1,202
TCDRS	317,550	268,726	84.62%	250,446
FICA	102,241	82,821	81.01%	78,784
FICA MED	31,900	28,152	88.25%	26,572
Health Insurance Expense	193,060	174,261	90.26%	174,106
Life Insurance Expense	5,874	2,550	43.41%	3,605

Central Texas Regional Mobility Authority
Income Statement
All Operating Departments

Account Name	Budget Amount FY 2014	Actual Year to Date 6/30/2014	Percent of Budget	Actual Prior Year to Date 6/30/2013
Auto Allowance Expense	10,200	5,100	50.00%	-
Other Benefits	190,261	88,478	46.50%	82,905
Unemployment Taxes	12,960	4,074	31.44%	(15)
Salary Reserve	50,000	-	0.00%	-
Total Salaries and Wages	3,119,051	2,585,895	82.91%	2,451,765
Contractual Services				
<u>Professional Services</u>				
Accounting	12,000	9,546	79.55%	9,153
Auditing	65,000	54,675	84.12%	47,570
Communications Consultant	-	3,000		-
Engineering	-	-	0.00%	346
General Engineering Consultant	460,000	13,454	2.92%	89,646
GEC-Trust Indenture Support	75,000	110,102	146.80%	87,480
GEC-Financial Planning Support	50,000	50,021	100.04%	53,266
GEC-Toll Ops Support	5,000	879	17.59%	8,632
GEC-Roadway Ops Support	325,000	378,810	116.56%	377,763
GEC-Technology Support	50,000	98,639	197.28%	49,766
GEC-Public Information Support	10,000	461	4.61%	7,673
GEC-General Support	275,000	297,183	108.07%	267,727
General System Consultant	175,000	137,392	78.51%	19,317
Image Processing - 183A	1,140,000	1,182,381	103.72%	1,118,450
Image Processing - Manor	120,000	319,603	266.34%	33,653
Facility maintenance	-	6,104		15,517
HERO	1,629,000	1,247,041	76.55%	1,271,959
Special Projects	-	526,372		109,899
Human Resources	50,000	7,110	14.22%	36,334
Legal	250,000	208,326	83.33%	358,161
Photography	10,000	9,146	91.46%	-
Traffic and Revenue Consultant	5,000	56,103	1122%	22,324
Communications and Marketing	-	-	0.00%	279,900
Total Professional Services	4,706,000	4,716,351	100.22%	4,264,537
<u>Other Contractual Services</u>				
IT Services	63,000	46,430	73.70%	40,806

Central Texas Regional Mobility Authority
Income Statement
All Operating Departments

Account Name	Budget Amount FY 2014	Actual Year to Date 6/30/2014	Percent of Budget	Actual Prior Year to Date 6/30/2013
Graphic Design Services	40,000	20,271	50.68%	11,070
Website Maintenance	35,000	45,850	131.00%	2,929
Research Services	50,000	9,301	18.60%	3,154
Copy Machine	10,000	10,768	107.68%	7,661
Software Licenses	17,200	18,958	110.22%	19,968
ETC Maintenance Contract	1,291,625	1,267,863	98.16%	1,047,738
ETC Development	125,000	-	0.00%	-
ETC Testing	30,000	-	0.00%	-
Communications and Marketing	140,000	271,567	193.98%	204
Advertising Expense	60,000	46,819	78.03%	70,242
Direct Mail	5,000	355	7.10%	757
Video Production	20,000	6,704	33.52%	20,920
Radio	10,000	-	0.00%	-
Other Public Relations	2,500	-	0.00%	-
Law Enforcement	250,000	259,839	103.94%	173,181
Special assignments	5,000	-	0.00%	-
Traffic Management	-	-	0.00%	42,823
Emergency Maintenance	10,000	-	0.00%	-
Generator Maintenance	20,000	5,590	27.95%	-
Generator Fuel	9,000	2,360	26.22%	-
Fire and Burglar Alarm	3,660	539	14.74%	114
Elevator Maintenance	2,640	3,037	115.03%	-
Refuse	780	574	73.56%	-
Pest Control	1,536	3,540	230.47%	-
Custodial	4,440	1,860	41.89%	-
Roadway Maintenance - 183A	750,000	545,646	72.75%	443,650
Roadway Maintenance - 290	-	34,574		-
Landscape Maintenance	250,000	107,453	42.98%	191,632
Signal & Illumination Maint	-	194,999		46,743
Mowing and litter control	-	-	0.00%	40,806
Graffiti removal	-	-	0.00%	225
Cell Phones	10,000	12,409	124.09%	9,684
Local Telephone Service	25,000	15,278	61.11%	14,628
Internet	6,000	1,098	18.30%	-
Fiber Optic System	30,000	83,014	276.71%	43,869
Other Communication Expenses	1,000	5,110	511.00%	127
Subscriptions	1,850	1,588	85.82%	1,038

**Central Texas Regional Mobility Authority
Income Statement
All Operating Departments**

Account Name	Budget Amount FY 2014	Actual Year to Date 6/30/2014	Percent of Budget	Actual Prior Year to Date 6/30/2013
Memberships	34,600	32,142	92.89%	28,625
Continuing Education	7,300	596	8.16%	6,178
Professional Development	14,000	501	3.58%	260
Seminars and Conferences	32,000	24,617	76.93%	21,544
Staff-Travel	89,000	76,828	86.32%	65,439
Other Contractual Svcs	200	-	0.00%	-
Tag Collection Fees	2,013,000	2,004,465	99.58%	1,505,249
Court Enforcement Costs	15,000	11,150	74.33%	-
Contractual Contingencies	130,500	34,114	26.14%	1,548
Total Other Contractual Services	5,615,831	5,207,805	92.73%	3,862,812
Total Contractual Services	10,321,831	9,924,157	96.15%	8,127,349
Materials and Supplies				
Books & Publications	6,500	2,249	34.61%	4,573
Office Supplies	10,000	18,683	186.83%	5,494
Computer Supplies	12,500	12,338	98.70%	12,988
Copy Supplies	2,200	1,275	57.93%	1,387
Annual Report printing	7,000	968	13.83%	5,534
Other Reports-Printing	10,000	4,689	46.89%	3,408
Direct Mail Printing	5,000	8,343	166.86%	-
Office Supplies-Printed	2,500	893	35.70%	4,595
Maintenance Supplies	-	400		-
Maintenance Supplies-Roadway	9,175	-	0.00%	-
Promotional Items	10,000	2,214	22.14%	4,827
Displays	5,000	-	0.00%	-
ETC spare parts expense	30,000	20,988	69.96%	-
Tools & Equipment Expense	1,000	43	4.29%	-
Misc Materials & Supplies	3,000	1,122	37.39%	288
Total Materials and Supplies	113,875	74,202	65.16%	43,094
Operating Expenses				
Gasoline	5,500	3,073	55.88%	3,412
Mileage Reimbursement	6,750	6,931	102.69%	5,722
Toll Tag Expense	2,700	499	18.48%	1,783

Central Texas Regional Mobility Authority
Income Statement
All Operating Departments

Account Name	Budget Amount FY 2014	Actual Year to Date 6/30/2014	Percent of Budget	Actual Prior Year to Date 6/30/2013
Parking	3,175	2,696	84.92%	32,526
Meeting Facilities	250	-	0.00%	-
Community Meeting/ Events	5,000	-	0.00%	-
Meeting Expense	17,300	8,675	50.14%	9,775
Public Notices	2,000	100	5.00%	-
Postage Expense	5,650	606	10.72%	473
Overnight Delivery Services	1,700	282	16.62%	307
Local Delivery Services	1,150	324	28.14%	12
Insurance Expense	90,000	96,825	107.58%	67,099
Repair & Maintenance-General	500	2,032	406.46%	956
Repair & Maintenance-Vehicles	500	1,052	210.34%	237
Repair & Maintenance Toll Equip	5,000	170	3.40%	400
Rent Expense	400,000	318,175	79.54%	200,908
Water	7,500	5,286	70.47%	5,926
Electricity	180,000	104,270	57.93%	74,132
Other Licenses	700	746	106.64%	729
Community Initiative Grants	65,000	55,000	84.62%	50,000
Non Cash Operating Expenses				
Amortization Expense	25,000	117,347	469.39%	264,757
Amort Expense - Refund Savings	-	1,027,860		128,482
Dep Exp- Furniture & Fixtures	14,000	-	0.00%	11,909
Dep Expense - Equipment	17,000	20,313	119.49%	17,834
Dep Expense - Autos & Trucks	7,000	6,898	98.55%	6,898
Dep Expense-Buildng & Toll Fac	100,000	177,115	177.11%	168,575
Dep Expense-Highways & Bridges	9,000,000	9,654,977	107.28%	7,640,140
Dep Expense-Communic Equip	175,000	196,115	112.07%	183,325
Dep Expense-Toll Equipment	986,000	1,646,173	166.95%	995,496
Dep Expense - Signs	175,000	249,402	142.52%	150,873
Dep Expense-Land Improvemts	160,000	557,901	348.69%	150,668
Depreciation Expense-Computers	11,000	26,257	238.70%	11,309
Total Operating Expenses	11,470,375	14,287,099	124.56%	10,184,665
Financing Expenses				
Arbitrage Rebate Calculation	6,000	6,630	110.50%	5,605
Loan Fee Expense	5,000	-	0.00%	5,000

**Central Texas Regional Mobility Authority
Income Statement
All Operating Departments**

Account Name	Budget Amount FY 2014	Actual Year to Date 6/30/2014	Percent of Budget	Actual Prior Year to Date 6/30/2013
Rating Agency Expense	50,000	37,000	74.00%	40,300
Trustee Fees	8,000	19,470	243.38%	6,500
Bank Fee Expense	8,000	5,354	66.93%	9,286
Continuing Disclosure	4,000	7,623	190.56%	-
Interest Expense	20,796,755	17,811,535	85.65%	22,573,480
Contingency	15,000	-	0.00%	-
 <u>Non Cash Financing Expenses</u>				
Bond issuance expense	400,000	217,829	54.46%	3,948,032
Total Financing Expenses	21,292,755	18,105,441	85.03%	26,588,203
 Other Gains or Losses				
Total Other Gains or Losses	-	-	0.00%	-
Total Expenses	\$ 46,317,887	\$ 44,976,794	97.10%	\$ 47,395,077
 Net Income	 \$ (5,859,102)	 \$ 110,324,638		 \$ 77,655,378

INVESTMENTS by FUND

		Balance June 30, 2014	
Renewal & Replacement Fund			
TexSTAR	2,911,422.14		TexSTAR 11,769,105.39
Regions Sweep	587,799.73		CD's 5,000,000.00
Agencies		3,499,221.87	Regions Sweep 193,286,098.78
TxDOT Grant Fund			Agencies 48,934,074.34
TexSTAR	82,189.69		
Regions Sweep	3,669,029.45		
CD's			
Agencies	5,734,759.15	9,485,978.29	\$ 258,989,278.51
Senior Debt Service Reserve Fund			
TexSTAR	589,997.35		
Regions Sweep	14,581,495.29		
Agencies	33,062,805.26	48,234,297.90	
2010 Senior Lien DSF			
Regions Sweep	1,792,469.20		
TexSTAR	-	1,792,469.20	
2011 Debt Service Acct			
Regions Sweep	8,949,603.87	8,949,603.87	
2013 Sr Debt Service Acct			
Regions Sweep	5,387,353.33	5,387,353.33	
2013 Sub Debt Serrvice Account			
Regions Sweep	3,178,586.05	3,178,586.05	
2010 Senior Lien DSRF			
Regions Sweep	-	-	
2011 Sub Debt DSRF			
Regions Sweep	2,025,263.81		
CD's	5,000,000.00	7,025,263.81	
2011 Sub DSF			
Regions Sweep	2,363,365.84	2,363,365.84	
Operating Fund			
TexSTAR	60,035.78		
TexSTAR-Trustee	3,169,072.76		
Regions Sweep	-	3,229,108.54	
Revenue Fund			
TexSTAR	1.00		
Regions Sweep	2,212,870.18	2,212,871.18	
General Fund			
TexSTAR	53.78		
Regions Sweep	12,498,522.10		
Agencies	5,011,226.83	17,509,802.71	
2013 Sub Debt Service Reserve Fund			
Regions Sweep	3,279,220.52		
Agencies	5,125,283.10	8,404,503.62	
MoPac Construction Fund			
Regions Sweep	78,319,497.05	78,319,497.05	
2010-1 Sub Lien Projects Fund			
TexSTAR	785,538.35		
Regions Sweep	-	785,538.35	
2010 Senior Lien Construction Fund			
TexSTAR	1.19		
Regions Sweep	181,751.05	181,752.24	
2011 Sub Debt Project fund			
TexSTAR	4,170,673.69		
Agencies			
Regions Sweep	32,874,887.19	37,045,560.88	
2011 Sr Financial Assistance Fund			
Regions Sweep	18,585,499.93	18,585,499.93	
2011 Senior Lien Project Fund			
TexSTAR	119.66		
Regions Sweep	298,849.94		
Agencies		298,969.60	
45SW Trust Account Travis County			
Regions Sweep	2,500,034.25	2,500,034.25	
		<u>\$ 258,989,278.51</u>	

CTRMA INVESTMENT REPORT

Month Ending 6/30/14						
Balance 6/1/2014	Additions	Discount Amortization	Accrued Interest	Withdrawals	Balance 6/30/2014	Rate Jun 14
Amount in Trustee TexStar						
2011 Sub Lien Construction Fund	4,170,563.43		110.26		4,170,673.69	0.035%
2011 Senior Lien Construction Fund	119.66				119.66	0.035%
2010 Senior Lien Construction Fund	1.19				1.19	0.035%
2010-1 Sub Lien Projects	792,775.43		20.88	7,257.96	785,538.35	0.035%
General Fund	53.78				53.78	0.035%
Trustee Operating Fund	3,068,987.51	1,100,000.00	85.25	1,000,000.00	3,169,072.76	0.035%
Renewal and Replacement	2,911,345.18		76.96		2,911,422.14	0.035%
TxDOT Grant Fund	82,187.52		2.17		82,189.69	0.035%
Revenue Fund	1.00				1.00	0.035%
Senior Lien Debt Service Reserve Fund	589,981.73		15.62		589,997.35	0.035%
11,616,016.43	1,100,000.00		311.14	1,007,257.96	11,709,069.61	
Amount in TexStar Operating Fund						
560,033.41	1,000,000.00		2.37	1,500,000.00	60,035.78	0.035%
Regions Sweep Money Market Fund						
Operating Fund	0.00	1,100,000.00		1,100,000.00	0.00	0.100%
45SW Trust Account Travis County	0.00	2,500,000.00	34.25		2,500,034.25	0.100%
2010 Senior Lien Project Acct	125,824.57	55,915.79	10.69		181,751.05	0.100%
2010-1 Sub Lien Projects Fund	0.00	7,257.96		7,257.96	0.00	0.100%
2011 Sub Lien Project Acct	33,371,428.05		2,834.29	499,375.15	32,874,887.19	0.100%
2011 Senior Lien Project Acct	298,612.19		237.75		298,849.94	0.100%
2011 Sr Financial Assistance Fund	7,859,832.38	10,725,000.00	667.55		18,585,499.93	0.100%
2010 Senior DSF	1,493,762.12	298,591.67	115.41		1,792,469.20	0.100%
2011 Senior Lien Debt Service Acct	8,892,255.42	56,593.91	754.54		8,949,603.87	0.100%
2011 Sub Debt Service Fund	2,362,714.04	451.13	200.67		2,363,365.84	0.100%
2013 Senior Lien Debt Service Acct	4,489,593.94	897,412.50	346.89		5,387,353.33	0.100%
2013 Subordinate Debt Service Acct	2,648,906.38	529,475.00	204.67		3,178,586.05	0.100%
2011 Sr Cap I Fund	38,580.07		3.28	38,583.35	0.00	0.100%
2011 Sub Debt CAP I	451.09		0.04	451.13	0.00	0.100%
TxDOT Grant Fund	3,668,717.86		311.59		3,669,029.45	0.100%
Renewal and Replacement	600,199.59		50.98	12,450.84	587,799.73	0.100%
Revenue Fund	1,636,524.61	4,482,227.05	152.10	3,906,033.58	2,212,870.18	0.100%
General Fund	12,103,625.31	1,602,887.76	1,010.40	1,209,001.37	12,498,522.10	0.100%
2011 Sub Debt Service Reserve Fund	2,025,091.82		171.99		2,025,263.81	0.100%
Senior Lien Debt Service Reserve Fund	14,505,263.34		76,231.95		14,581,495.29	0.100%
2013 Sub Debt Service Reserve Fund	3,278,942.03		278.49		3,279,220.52	0.100%

CTRMA INVESTMENT REPORT

Month Ending 6/30/14						
Balance 6/1/2014	Additions	Discount Amortization	Accrued Interest	Withdrawals	Balance 6/30/2014	
67,632,786.32	16,500,000.00		5,838.91	5,819,128.18	78,319,497.05	
167,033,111.13	38,755,812.77	0.00	89,456.44	12,592,281.56	193,286,098.78	
Amount in Fed Agencies and Treasuries						
Amortized Principal	48,958,438.14	(24,363.80)			48,934,074.34	
Accrued Interest			39,285.00			
	48,958,438.14	0.00	(24,363.80)	0.00	48,934,074.34	
Certificates of Deposit						
Total in Pools	5,000,000.00				5,000,000.00	
Total in Money Market	12,176,049.84	2,100,000.00	313.51	2,507,257.96	11,769,105.39	
Total in Fed Agencies	167,033,111.13	38,755,812.77	89,456.44	12,592,281.56	193,286,098.78	
	48,958,438.14	0.00	(24,363.80)	0.00	48,934,074.34	
Total Invested	233,167,599.11	40,855,812.77	(24,363.80)	89,769.95	15,099,539.52	258,989,278.51

MoPac Managed Lane Construction Fund

Rate
Jun 14
0.100%

Amount in Fed Agencies and Treasuries

Amortized Principal

Accrued Interest

Certificates of Deposit

Total in Pools

Total in Money Market

Total in Fed Agencies

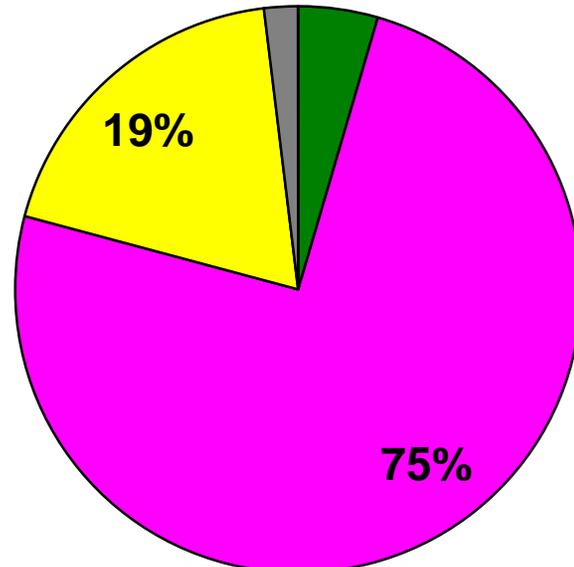
Total Invested

All Investments in the portfolio are in compliance with the CTRMA's Investment policy.

William Chapman, CFO

Cindy Demers, Controller

Allocation of Funds 4%



■ Total in Pools

■ Total in Money Market

■ Total in Fed Agencies

■ Total in CD's

Amount of investments As of June 30, 2014

Agency	CUSIP #	COST	Book Value	Market Value	Yield to Maturity	Purchased	Matures	FUND
	313378LX7	4,013,754.20	4,008,596.39	4,009,960.00	0.0267%	1/9/2014	4/30/2015	General
Federal Home Loan Bank	313378M57	1,004,065.22	1,002,630.44	1,003,150.00	0.0028%	1/9/2014	5/29/2015	General
Freddie Mac	3137EADD8	1,004,940.00	1,001,646.67	1,002,880.00	0.2290%	12/3/2012	4/17/2015	TxDOT Grant Fund
Northside ISD	66702RAG7	1,057,700.00	1,015,386.67	1,017,180.00	0.3580%	12/5/2012	2/15/2015	TxDOT Grant Fund
Federal Home Loan Bank	313371KG0	1,019,000.00	1,013,818.18	1,014,850.00	0.3912%	1/9/2014	10/28/2015	TxDOT Grant Fund
Fannie Mae	3135G0QB2	1,001,990.00	1,001,447.27	2,707,776.00	0.0381%	1/9/2014	10/22/2015	TxDOT Grant Fund
Fannie Mae	3135G0QB2	1,703,383.00	1,702,460.36		0.0381%	1/9/2014	10/22/2015	TxDOT Grant Fund
Fannie Mae	3135G0BY8	8,081,952.00	8,008,852.84	8,009,840.00	0.2150%	2/8/2013	8/28/2014	Senior DSRF
Federal Home Loan Bank	313371W51	12,217,422.00	12,054,355.50	12,060,360.00	0.2646%	2/8/2013	12/12/2014	Senior DSRF
Federal Home Loan Bank	3134G4T57	7,995,920.00	7,996,770.00	8,001,920.00	0.4750%	1/28/2014	1/28/2016	Senior DSRF
Fannie Mae	3135G0VA8	5,003,500.00	5,002,826.92	5,008,500.00	0.0468%	1/23/2014	3/1/3016	Senior DSRF
Federal Home Loan Bank	31398A3T7	5,164,996.34	5,125,283.10	5,128,932.48	0.3660%	1/9/2014	9/21/2015	2013 Sub DSRF
			48,934,074.34	48,965,348.48				

Agency	CUSIP #	COST	Cummulative Amortization	6/30/2014		Interest Income June 30, 2014		
				Book Value	Maturity Value	Accrued Interest	Amortization	Interest Earned
Federal Home Loan Bank	313378LX7	4,013,754.20	5,157.81	4,008,596.39	4,000,000.00	1,766.67	(859.64)	907.03
Federal Home Loan Bank	313378M57	1,004,065.22	1,434.78	1,002,630.44	1,000,000.00	475.00	(239.13)	235.87
Freddie Mac	3137EADD8	1,004,940.00	3,293.33	1,001,646.67	1,000,000.00	416.67	(164.67)	252.00
Northside ISD	66702RAG7	1,057,700.00	42,313.33	1,015,386.67	1,000,000.00	2,500.00	(1,923.33)	576.67
Federal Home Loan Bank	313371KG0	1,019,000.00	5,181.82	1,013,818.18	1,000,000.00	1,208.33	(863.64)	344.69
Fannie Mae	3135G0QB2	1,001,990.00	542.73	1,001,447.27	1,000,000.00	416.67	(90.45)	326.22
Fannie Mae	3135G0QB2	1,703,383.00	922.64	1,702,460.36	1,700,000.00	708.33	(153.77)	554.56
Fannie Mae	3135G0BY8	8,081,952.00	73,099.16	8,008,852.84	8,000,000.00	5,833.33	(4,426.43)	1,406.90
Federal Home Loan Bank	313371W51	12,217,422.00	163,066.50	12,054,355.50	12,000,000.00	12,500.00	(9,059.25)	3,440.75
Federal Home Loan Bank	3134G4T57	7,995,920.00	850.00	7,996,770.00	8,000,000.00	3,000.00	170.00	3,170.00
Fannie Mae	3135G0VA8	5,003,500.00	673.08	5,002,826.92	5,000,000.00	2,083.33	(134.62)	1,948.71
Federal Home Loan Bank	31398A3T7	5,164,996.34	39,713.24	5,125,283.10	5,026,000.00	8,376.67	(6,618.87)	1,757.80
		49,268,622.76	336,248.42	48,934,074.34	48,726,000.00	39,285.00	(24,363.80)	14,921.20

June 30, 2014

Certificates of Deposit Outstanding

Bank	CUSIP #	COST	Yield to Maturity	Purchased	Matures	June 30, 2014 Interest	FUND
Compass Bank	CD 02636	5,000,000	0.35%	2/5/2013	2/5/2015	\$ 1,458.33	2011 Sub DSRF
		<u>5,000,000</u>				<u>\$ 1,458.33</u>	

Travis County Escrow account

Balance		Accrued		Balance
6/1/2014	Additions	Interest	Withdrawals	6/30/2014
\$ 15,743,039.15		\$ 750.92	\$ 65,105.16	\$ 15,678,684.91



Monthly Newsletter - June 2014

Performance

As of June 30, 2014

Current Invested Balance	\$4,682,201,994.16
Weighted Average Maturity (1)	56 Days
Weighted Average Maturity (2)	84 Days
Net Asset Value	1.000038
Total Number of Participants	788
Management Fee on Invested Balance	0.05%*
Interest Distributed	\$349,405.62
Management Fee Collected	\$212,708.71
% of Portfolio Invested Beyond 1 Year	4.72%
Standard & Poor's Current Rating	AAAm

Rates reflect historical information and are not an indication of future performance.

June Averages

Average Invested Balance	\$5,175,770,526.01
Average Monthly Yield, on a simple basis	0.0322%
Average Weighted Average Maturity (1)*	50 Days
Average Weighted Average Maturity (2)*	76 Days

Definition of Weighted Average Maturity (1) & (2)

- (1) This weighted average maturity calculation uses the SEC Rule 2a-7 definition for stated maturity for any floating rate instrument held in the portfolio to determine the weighted average maturity for the pool. This Rule specifies that a variable rate instrument to be paid in 397 calendar days or less shall be deemed to have a maturity equal to the period remaining until the next readjustment of the interest rate.
- (2) This weighted average maturity calculation uses the final maturity of any floating rate instruments held in the portfolio to calculate the weighted average maturity for the pool.

* The maximum management fee authorized for the TexSTAR Cash Reserve Fund is 12 basis points. This fee June be waived in full or in part in the discretion of the TexSTAR co-administrators at any time as provided for in the TexSTAR Information Statement.

New Participants

We would like to welcome the following entities who joined the TexSTAR program in June:

★ City of Celina

★ Town of Trophy Club

Internet System Upgrade

Internet security is top of mind for any organization that provides internet access and continual review is necessary to keep up with the rapidly changing pace of technology. As a result of our ongoing efforts in this area, the TexSTAR website is migrating from Single Factor Authentication to SiteMinder Utility Multifactor Authentication (MFA) to enhance system security by introducing secondary authentication (security questions and device recognition) and enhancing participant access to password reset and other authentication-related self-service pages. When you log on to the TexSTAR internet transaction system on or after Monday, July 20th, you will be prompted to go through the process to establish your security questions. We are pleased to introduce this system upgrade as we strive to provide the highest level of services to our TexSTAR participants. Please watch your email this month for more information.

Economic Commentary

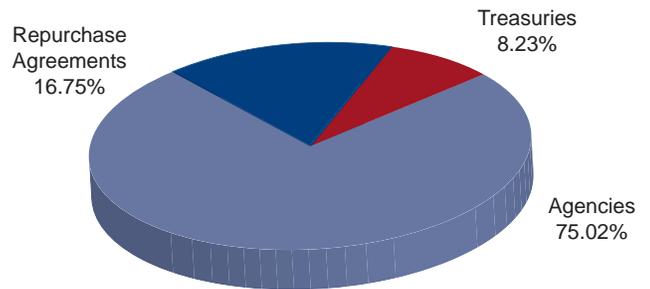
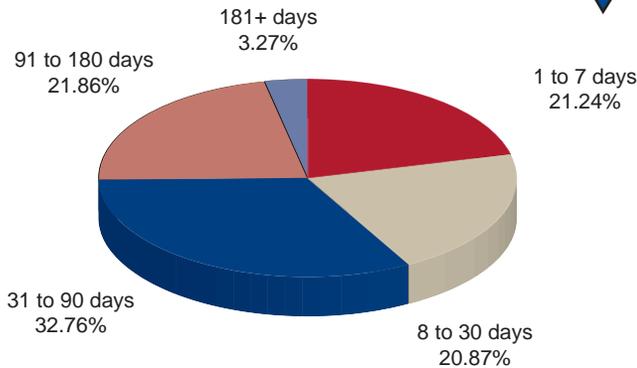
Fixed income, equities and commodities all performed well in the second quarter. This was primarily driven by the accommodative stance of central banks, which have maintained or eased their policies in an environment of low growth, low inflation and low volatility. In the U.S., the Federal Open Market Committee (FOMC) met twice in the second quarter, and largely confirmed market expectations, reducing asset purchases by \$10 billion at each meeting to \$35 billion beginning in July. The FOMC also revised their economic and rate expectations, downgrading 2014 GDP projections after a poor first quarter, shifting their unemployment rate forecast lower and marginally raising their inflation projections. Chairwoman Janet Yellen confirmed the Fed's commitment to maintaining low rates for as long as needed to support economic growth. However, policymakers including Chairwoman Yellen, have warned investors not to become complacent about the current low volatility environment as policy normalizes once growth improves further. Data released in the second quarter was stronger than the first quarter. Labor markets continued to show steady improvement, inflation appeared to have bottomed, and housing data rebounded after a soft six months. It is believed the Fed will provide the necessary liquidity to allow the economy to expand until it sees broader housing strength and/or material wage inflation, as it recognizes the potential risk of its first increase in the fed funds rate in over seven years. The sequencing of Fed policy normalization now appears to involve the continued reinvestment of principal and interest on its existing mortgage holdings and a more nuanced management of its lending rates, through interest on excess reserves, and its reverse repo facility.

This information is an excerpt from an economic report dated June 2014 provided to TexSTAR by JP Morgan Asset Management, Inc., the investment manager of the TexSTAR pool.

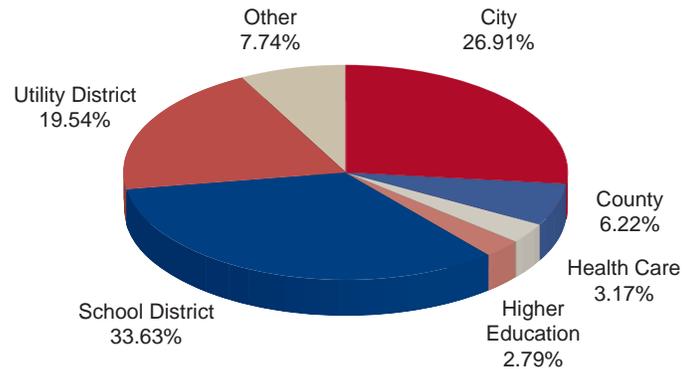
For more information about TexSTAR, please visit our web site at www.texstar.org.

Information at a Glance

Portfolio by Type of Investment As of June 30, 2014



Portfolio by Maturity As of June 30, 2014



Distribution of Participants by Type As of June 30, 2014

Historical Program Information

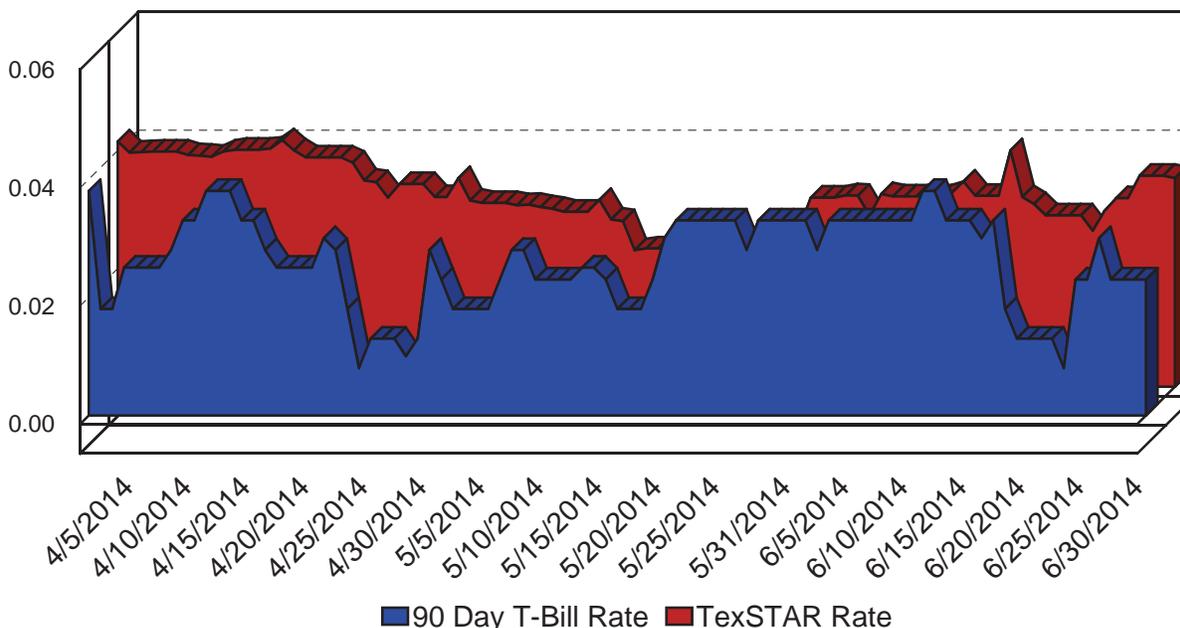
Month	Average Rate	Book Value	Market Value	Net Asset Value	WAM (1)*	WAM (2)*	Number of Participants
Jun 14	0.0322%	\$4,682,201,994.16	\$4,682,381,855.14	1.000038	50	76	788
May 14	0.0273%	5,188,136,060.86	5,188,307,944.39	1.000034	52	74	786
Apr 14	0.0379%	5,297,751,521.64	5,298,035,810.85	1.000053	51	71	784
Mar 14	0.0400%	5,447,221,784.71	5,447,546,676.56	1.000059	51	66	784
Feb 14	0.0318%	5,890,162,246.46	5,890,513,830.50	1.000066	49	65	783
Jan 14	0.0303%	5,518,659,649.58	5,518,895,897.21	1.000048	49	64	781
Dec 13	0.0357%	4,749,571,555.83	4,749,808,699.35	1.000050	52	65	781
Nov 13	0.0405%	4,358,778,907.03	4,358,933,052.64	1.000035	52	63	781
Oct 13	0.0434%	4,549,543,382.92	4,549,816,768.31	1.000060	52	63	781
Sep 13	0.0390%	4,545,216,845.55	4,545,590,808.40	1.000082	52	64	781
Aug 13	0.0474%	4,682,919,318.35	4,683,351,916.02	1.000091	52	59	777
Jul 13	0.0487%	4,833,856,137.70	4,834,318,370.27	1.000095	52	56	776

Portfolio Asset Summary as of June 30, 2014

	Book Value	Market Value
Uninvested Balance	\$ 368.80	\$ 368.80
Accrual of Interest Income	1,542,915.18	1,542,915.18
Interest and Management Fees Payable	(465,686.31)	(465,686.31)
Payable for Investment Purchased	0.00	0.00
Repurchase Agreement	784,233,000.00	784,233,000.00
Government Securities	3,896,891,396.49	3,897,071,257.47
Total	\$ 4,682,201,994.16	\$ 4,682,381,855.14

Market value of collateral supporting the Repurchase Agreements is at least 102% of the Book Value. The portfolio is managed by J.P. Morgan Chase & Co. and the assets are safekept in a separate custodial account at the Federal Reserve Bank in the name of TexSTAR. The only source of payment to the Participants are the assets of TexSTAR. There is no secondary source of payment for the pool such as insurance or guarantee. Should you require a copy of the portfolio, please contact TexSTAR Participant Services.

TexSTAR versus 90-Day Treasury Bill



This material is for information purposes only. This information does not represent an offer to buy or sell a security. The above rate information is obtained from sources that are believed to be reliable; however, its accuracy or completeness June be subject to change. The TexSTAR management fee may be waived in full or in part at the discretion of the TexSTAR co-administrators and the TexSTAR rate for the period shown reflects waiver of fees. This table represents historical investment performance/return to the customer, net of fees, and is not an indication of future performance. An investment in the security is not insured or guaranteed by the Federal Deposit Insurance Corporation or any other government agency. Although the issuer seeks to preserve the value of an investment at \$1.00 per share, it is possible to lose money by investing in the security. Information about these and other program details are in the fund's Information Statement which should be read carefully before investing. The yield on the 90-Day Treasury Bill ("T-Bill Yield") is shown for comparative purposes only. When comparing the investment returns of the TexSTAR pool to the T-Bill Yield, you should know that the TexSTAR pool consist of allocations of specific diversified securities as detailed in the respective Information Statements. The T-Bill Yield is taken from Bloomberg Finance L.P. and represents the daily closing yield on the then current 90-day T-Bill.

Daily Summary for June 2014

Date	Mny Mkt Fund Equiv. [SEC Std.]	Daily Allocation Factor	TexSTAR Invested Balance	Market Value Per Share	WAM Days (1)*	WAM Days (2)*
6/1/2014	0.0320%	0.000000876	\$5,188,136,060.86	1.000034	51	77
6/2/2014	0.0324%	0.000000887	\$5,190,358,123.28	1.000028	51	77
6/3/2014	0.0323%	0.000000886	\$5,213,771,614.67	1.000027	51	77
6/4/2014	0.0290%	0.000000794	\$5,123,026,893.18	1.000025	51	78
6/5/2014	0.0326%	0.000000892	\$5,179,585,795.13	1.000026	50	77
6/6/2014	0.0322%	0.000000882	\$5,180,121,707.91	1.000027	48	74
6/7/2014	0.0322%	0.000000882	\$5,180,121,707.91	1.000027	48	74
6/8/2014	0.0322%	0.000000882	\$5,180,121,707.91	1.000027	48	74
6/9/2014	0.0322%	0.000000882	\$5,142,084,002.90	1.000026	48	74
6/10/2014	0.0322%	0.000000882	\$5,170,410,652.35	1.000026	48	74
6/11/2014	0.0328%	0.000000899	\$5,157,885,024.42	1.000041	48	73
6/12/2014	0.0347%	0.000000950	\$5,119,529,240.31	1.000032	51	78
6/13/2014	0.0323%	0.000000885	\$5,180,066,773.38	1.000028	49	76
6/14/2014	0.0323%	0.000000885	\$5,180,066,773.38	1.000028	49	76
6/15/2014	0.0323%	0.000000885	\$5,180,066,773.38	1.000028	49	76
6/16/2014	0.0400%	0.000001095	\$5,317,210,943.53	1.000024	47	74
6/17/2014	0.0321%	0.000000880	\$5,315,117,686.38	1.000023	48	74
6/18/2014	0.0309%	0.000000847	\$5,325,313,635.18	1.000019	49	75
6/19/2014	0.0290%	0.000000794	\$5,328,758,398.39	1.000032	48	74
6/20/2014	0.0290%	0.000000794	\$5,289,808,735.32	1.000031	47	73
6/21/2014	0.0290%	0.000000794	\$5,289,808,735.32	1.000031	47	73
6/22/2014	0.0290%	0.000000794	\$5,289,808,735.32	1.000031	47	73
6/23/2014	0.0264%	0.000000722	\$5,320,635,163.17	1.000028	46	71
6/24/2014	0.0297%	0.000000815	\$5,232,252,152.65	1.000034	53	79
6/25/2014	0.0319%	0.000000874	\$5,214,853,271.22	1.000032	53	79
6/26/2014	0.0319%	0.000000875	\$5,127,371,829.43	1.000036	54	80
6/27/2014	0.0357%	0.000000978	\$4,991,540,549.72	1.000034	53	80
6/28/2014	0.0357%	0.000000978	\$4,991,540,549.72	1.000034	53	80
6/29/2014	0.0357%	0.000000978	\$4,991,540,549.72	1.000034	53	80
6/30/2014	0.0353%	0.000000968	\$4,682,201,994.16	1.000038	56	84
Average	0.0322%	0.000000881	\$5,175,770,526.01		50	76

TexSTAR Participant Services
First Southwest Asset Management, Inc.
325 North St. Paul Street, Suite 800
Dallas, Texas 75201



TexSTAR Board Members

<i>William Chapman</i>	<i>Central Texas Regional Mobility Authority</i>	<i>Governing Board President</i>
<i>Nell Lange</i>	<i>City of Frisco</i>	<i>Governing Board Vice President</i>
<i>Kenneth Huewitt</i>	<i>Houston ISD</i>	<i>Governing Board Treasurer</i>
<i>Michael Bartolotta</i>	<i>First Southwest Company</i>	<i>Governing Board Secretary</i>
<i>Joni Freeman</i>	<i>JP Morgan Chase</i>	<i>Governing Board Asst. Sec./Treas.</i>
<i>Eric Cannon</i>	<i>Town of Addison</i>	<i>Advisory Board</i>
<i>Nicole Conley</i>	<i>Austin ISD</i>	<i>Advisory Board</i>
<i>Pamela Moon</i>	<i>City of Lubbock</i>	<i>Advisory Board</i>
<i>Monte Mercer</i>	<i>North Central TX Council of Government</i>	<i>Advisory Board</i>
<i>Oscar Cardenas</i>	<i>Northside ISD</i>	<i>Advisory Board</i>
<i>Stephen Fortenberry</i>	<i>Plano ISD</i>	<i>Advisory Board</i>
<i>Becky Brooks</i>	<i>Government Resource Associates, LLC</i>	<i>Advisory Board</i>

For more information contact TexSTAR Participant Services ★ 1-800-TEX-STAR ★ www.texstar.org



J.P.Morgan
Asset Management



**CENTRAL TEXAS
Regional Mobility Authority**

AGENDA ITEM #7 SUMMARY

Approve an amendment to the contract with CDM Smith Inc. for traffic and revenue studies on Mobility Authority toll projects.

Strategic Plan Relevance: Regional Mobility

Department: Finance

Associated Costs: N/A

Funding Source: N/A

Board Action Required: Yes

Description of Matter:

This amendment replaces the July 31, 2014, expiration date in the current Agreement for Traffic and Revenue Engineering Services with CDM Smith Inc. to provide that the contract will continue until either party provides a 120 day notice of termination, or terminates the contract under another provision of the current contract.

Reference documentation: Draft Resolution

Contact for further information: Bill Chapman, Chief Financial Officer
Cindy Demers, Controller

**GENERAL MEETING OF THE BOARD OF DIRECTORS
OF THE
CENTRAL TEXAS REGIONAL MOBILITY AUTHORITY**

RESOLUTION NO. 14-___

**APPROVING AN AMENDMENT TO THE CONTRACT
WITH CDM SMITH INC. FOR TRAFFIC AND REVENUE STUDIES
ON MOBILITY AUTHORITY TOLL PROJECTS.**

WHEREAS, CDM Smith Inc. provides traffic and revenue studies to the Mobility Authority under that certain “Agreement for Traffic and Revenue Engineering Services” executed to be effective August 1, 2009 (the “Agreement”); and

WHEREAS, by its terms, the Agreement will terminate on July 31, 2014; and

WHEREAS, the Executive Director recommends extending the agreement with CDM Smith Inc. to provide traffic and revenue engineering services as may be requested from time-to-time by the Mobility Authority by executing the amendment in the form or substantially in the form set forth in the attached as Exhibit 1.

NOW THEREFORE, BE IT RESOLVED, that the Board of Directors hereby approves the proposed amendment and authorizes the Executive Director to execute on behalf of the Mobility Authority the amendment in the form or substantially in the form of the “2014 Amendment” attached as Exhibit 1.

Adopted by the Board of Directors of the Central Texas Regional Mobility Authority on the 30th day of July, 2014.

Submitted and reviewed by:

Approved:

Andrew Martin
General Counsel for the Central
Texas Regional Mobility Authority

Ray A. Wilkerson
Chairman, Board of Directors
Resolution Number: 14-___
Date Passed: 7/30/14

EXHIBIT 1 TO RESOLUTION 14-

AMENDMENT TO AGREEMENT WITH CDM SMITH INC.

**2014 Amendment
to the
Traffic and Revenue Engineering Services Agreement**

This 2014 Amendment to the “Agreement for Traffic and Revenue Engineering Services” by and between the Central Texas Regional Mobility Authority (the “CTRMA” or “Authority”) and CDM Smith Inc., a Massachusetts corporation, as successor in interest to Wilbur Smith Associates Inc. (the “Consultant”) effective August 1, 2009 (the “Agreement”) is executed to be effective on the 30th day of July, 2014.

The Authority and Consultant hereby agree as follows:

Article 4 is amended to read in its entirety as follows:

ARTICLE 4. TERM OF PERFORMANCE

This Agreement shall remain in full force and effect until terminated under this Article 4 or as provided in another Article of this Agreement.

The CTRMA or the Consultant may terminate this Agreement by providing a written notice of termination no later than 120 days before the effective termination date established by that written notice.

If at any time during the contract term the Consultant cannot provide the requested Services within the time required by the CTRMA or for any other reason, the Authority reserves the unilateral right to procure the Services from any other source it deems capable of providing those Services.

The parties are signing this amendment to be effective on the date stated in the introductory clause.

CENTRAL TEXAS REGIONAL
AUTHORITY

CDM SMITH INC.

Mike Heiligenstein,
Executive Director

Sean Tenney
Vice President



**CENTRAL TEXAS
Regional Mobility Authority**

AGENDA ITEM #8 SUMMARY

Authorize the Executive Director to execute an amendment to the advance funding agreement with the Texas Department of Transportation for the Bergstrom Expressway Project.

Strategic Plan Relevance:	Regional Mobility
Department:	Engineering
Associated Costs:	\$13,730,000
Funding Source:	Amendment to TxDOT Advance Funding Agreement
Board Action Required:	Yes

Description of Matter: This amendment will add \$13,730,000 to the previously approved Advance Funding Agreement of \$6,500,000 making a total of \$20,230,000 in STP MM funds to the Mobility Authority to cover project development costs for the Bergstrom Expressway Project. These efforts will include, but not be limited to, Project management and oversight, utilities, right of way, administrative tasks, community outreach, and preliminary and final design. Any unused funds can be carried over to cover construction costs.

Reference documentation: Draft Resolution
Advance Funding Agreement Amendment #1

Contact for further information: Wesley M. Burford, P.E., Director of Engineering

**GENERAL MEETING OF THE BOARD OF DIRECTORS
OF THE
CENTRAL TEXAS REGIONAL MOBILITY AUTHORITY**

RESOLUTION NO. 14-___

**AUTHORIZING THE EXECUTIVE DIRECTOR TO EXECUTE AN AMENDMENT TO
THE ADVANCE FUNDING AGREEMENT WITH THE TEXAS DEPARTMENT OF
TRANSPORTATION FOR THE BERGSTROM EXPRESSWAY PROJECT.**

WHEREAS, the Central Texas Regional Mobility Authority (“Mobility Authority”) is empowered to study and develop potential transportation projects; and

WHEREAS, the Mobility Authority is considering a transportation project on U.S. Highway 183 South, commonly referred to as the Bergstrom Expressway Project (the “Project”); and

WHEREAS, the Board of Directors passed Resolution No. 13-066 on October 30, 2013, approving an advance funding agreement with the Texas Department of Transportation for the Bergstrom Expressway; and

WHEREAS, additional funding is needed for the Project and is available from the Texas Department of Transportation by amending the existing advance funding agreement; and

WHEREAS, the Executive Director recommends approval of the proposed amendment to the advance funding agreement in the form or substantially in the form attached as Exhibit 1.

NOW THEREFORE, BE IT RESOLVED, that the Board of Directors approves the amendment, and authorizes the Executive Director to finalize and execute the amendment to the advance funding agreement in the form or substantially in the form attached as Exhibit 1.

Adopted by the Board of Directors of the Central Texas Regional Mobility Authority on the 30th day of July, 2014.

Submitted and reviewed by:

Approved:

Andrew Martin
General Counsel for the Central
Texas Regional Mobility Authority

Ray A. Wilkerson
Chairman, Board of Directors
Resolution Number: 14-___
Date Passed: 7/30/14

Exhibit 1

Amendment to Advance Funding Agreement for Bergstrom Expressway

[on the following 6 pages]

STATE OF TEXAS §

COUNTY OF TRAVIS §

**ADVANCE FUNDING AGREEMENT
AMENDMENT #1**

THIS AMENDMENT is made by and between the State of Texas, acting through the Texas Department of Transportation, called the State, and the Central Texas Regional Mobility Authority, acting by and through its duly authorized officials, called the Local Government.

W I T N E S S E T H

WHEREAS, the State and the Local Government executed a contract on February 6 of 2014 to effectuate their agreement to complete a highway improvement generally described as construct main lanes (Toll) and frontage roads; and,

WHEREAS, it has become necessary to update the Federal and State provisions and requirement; and,

WHEREAS, it has become necessary to amend that contract;

NOW THEREFORE, in consideration of the premises and of the mutual covenants and agreements of the parties, the State and the Local Government do agree as follows:

A G R E E M E N T

1. Description of Amended Items

Under Agreement: Article 3. Local Project Sources and Uses of Funds

3.F is deleted in its entirety and replaced with the following:

Prior to the performance of any engineering review work by the State, the Local Government will pay to the State the amount specified in Attachment C. At a minimum, this amount shall equal the Local Government's funding share for the estimated cost of preliminary engineering for the Project. At least sixty (60) days prior to the date set for receipt of the construction bids, the Local Government shall remit its remaining financial share for the State's estimated construction oversight and construction cost.

3.L is deleted in its entirety and replaced with the following:

If the Project has been approved for a "fixed price" or a "periodic payment" non-standard funding or payment arrangement under 43 TAC §15.52, the budget in Attachment C will clearly state the amount of the fixed price or the periodic payment schedule.

Article 24, Disadvantaged Business Enterprise (DBE) Program Requirements, Article 25, Debarment Certifications, Article 28, Federal Funding Accountability and Transparency Act Requirements and Article 29, Single Audit Report is deleted in their entirety and replaced with the following:

24. Disadvantaged Business Enterprise (DBE) Program Requirements

- A. The parties shall comply with the Disadvantaged Business Enterprise Program requirements established in 49 CFR Part 26.
- B. The Local Government shall adopt, in its totality, the State's federally approved DBE program.
- C. The Local Government shall set an appropriate DBE goal consistent with the State's DBE guidelines and in consideration of the local market, project size, and nature of the goods or services to be acquired. The Local Government shall have final decision-making authority regarding the DBE goal and shall be responsible for documenting its actions.
- D. The Local Government shall follow all other parts of the State's DBE program referenced in TxDOT Form 2395, Memorandum of Understanding Regarding the Adoption of the Texas Department of Transportation's Federally-Approved Disadvantaged Business Enterprise by Entity, and attachments found at web address http://ftp.dot.state.tx.us/pub/txdot-info/bop/dbe/mou/mou_attachments.pdf.
- E. The Local Government shall not discriminate on the basis of race, color, national origin, or sex in the award and performance of any U.S. Department of Transportation (DOT)-assisted contract or in the administration of its DBE program or the requirements of 49 CFR Part 26. The Local Government shall take all necessary and reasonable steps under 49 CFR Part 26 to ensure non-discrimination in award and administration of DOT-assisted contracts. The State's DBE program, as required by 49 CFR Part 26 and as approved by DOT, is incorporated by reference in this agreement. Implementation of this program is a legal obligation and failure to carry out its terms shall be treated as a violation of this agreement. Upon notification to the Local Government of its failure to carry out its approved program, the State may impose sanctions as provided for under 49 CFR Part 26 and may, in appropriate cases, refer the matter for enforcement under 18 U.S.C. 1001 and the Program Fraud Civil Remedies Act of 1986 (31 U.S.C. 3801 et seq.).
- F. Each contract the Local Government signs with a contractor (and each subcontract the prime contractor signs with a sub-contractor) must include the following assurance: *The contractor, sub-recipient, or sub-contractor shall not discriminate on the basis of race, color, national origin, or sex in the performance of this contract. The contractor shall carry out applicable requirements of 49 CFR Part 26 in the award and administration of DOT-assisted contracts. Failure by the contractor to carry out these requirements is a material breach of this agreement, which may result in the termination of this agreement or such other remedy as the recipient deems appropriate.*

25. Debarment Certifications

The parties are prohibited from making any award at any tier to any party that is debarred or suspended or otherwise excluded from or ineligible for participation in Federal Assistance Programs under Executive Order 12549, "Debarment and Suspension." By executing this agreement, the Local Government certifies that it and its principals are not currently debarred, suspended, or otherwise excluded from or ineligible for participation in Federal Assistance Programs under Executive Order 12549 and further certifies that it will not do business with any party, to include principals, that is currently debarred, suspended, or otherwise excluded from or ineligible for participation in Federal Assistance Programs under Executive Order 12549. The parties to this contract shall require any party to a subcontract or purchase order awarded under this contract to certify its eligibility to receive federal funds and, when requested by the State, to furnish a copy of the certification.

28. Federal Funding Accountability and Transparency Act Requirements

- A. Any recipient of funds under this agreement agrees to comply with the Federal Funding Accountability and Transparency Act (FFATA) and implementing regulations at 2 CFR Part 170, including Appendix A. This agreement is subject to the following award terms:

<http://www.gpo.gov/fdsys/pkg/FR-2010-09-14/pdf/2010-22705.pdf> and
<http://www.gpo.gov/fdsys/pkg/FR-2010-09-14/pdf/2010-22706.pdf>.

B. The Local Government agrees that it shall:

1. Obtain and provide to the State a System for Award Management (SAM) number (Federal Acquisition Regulation, Part 4, Sub-part 4.11) if this award provides more than \$25,000 in Federal funding. The SAM number may be obtained by visiting the SAM website whose address is: <https://www.sam.gov/portal/public/SAM/>
2. Obtain and provide to the State a Data Universal Numbering System (DUNS) number, a unique nine-character number that allows Federal government to track the distribution of federal money. The DUNS may be requested free of charge for all businesses and entities required to do so by visiting the Dun & Bradstreet (D&B) on-line registration website <http://fedgov.dnb.com/webform>; and
3. Report the total compensation and names of its top five (5) executives to the State if:
 - i. More than 80% of annual gross revenues are from the Federal government, and those revenues are greater than \$25,000,000; and
 - ii. The compensation information is not already available through reporting to the U.S. Securities and Exchange Commission.

29. Single Audit Report

- A.** The parties shall comply with the requirements of the Single Audit Act of 1984, P.L. 98-502, ensuring that the single audit report includes the coverage stipulated in OMB Circular A-133.
- B.** If threshold expenditures of \$750,000 or more are met during the Local Government's fiscal year, the Local Government must submit a Single Audit Report and Management Letter (if applicable) to TxDOT's Audit Office, 125 E. 11th Street, Austin, TX 78701 or contact TxDOT's Audit Office at <http://www.txdot.gov/inside-txdot/office/audit/contact.html>.
- C.** If expenditures are less than \$750,000 during the Local Government's fiscal year, the Local Government must submit a statement to TxDOT's Audit Office as follows: "We did not meet the \$750,000 expenditure threshold and therefore, are not required to have a single audit performed for FY _____."
- D.** For each year the project remains open for federal funding expenditures, the Local Government will be responsible for filing a report or statement as described above. The required annual filing shall extend throughout the life of the agreement, unless otherwise amended or the project has been formally closed out and no charges have been incurred within the current fiscal year.

Attachment C, Project Budget is deleted in its entirety and replaced with Attachment C-1, Project Budget, contained herein. The Federal and State Participation is updated.

All other provisions of the original contract are unchanged and remain in full force and effect.

2. Signatory Warranty

Each signatory warrants that the signatory has necessary authority to execute this agreement on behalf of the entity represented.

THIS AGREEMENT IS EXECUTED by the State and the Local Government in duplicate.

THE LOCAL GOVERNMENT

Signature

Mike Heiligenstein

Typed or Printed Name

Executive Director

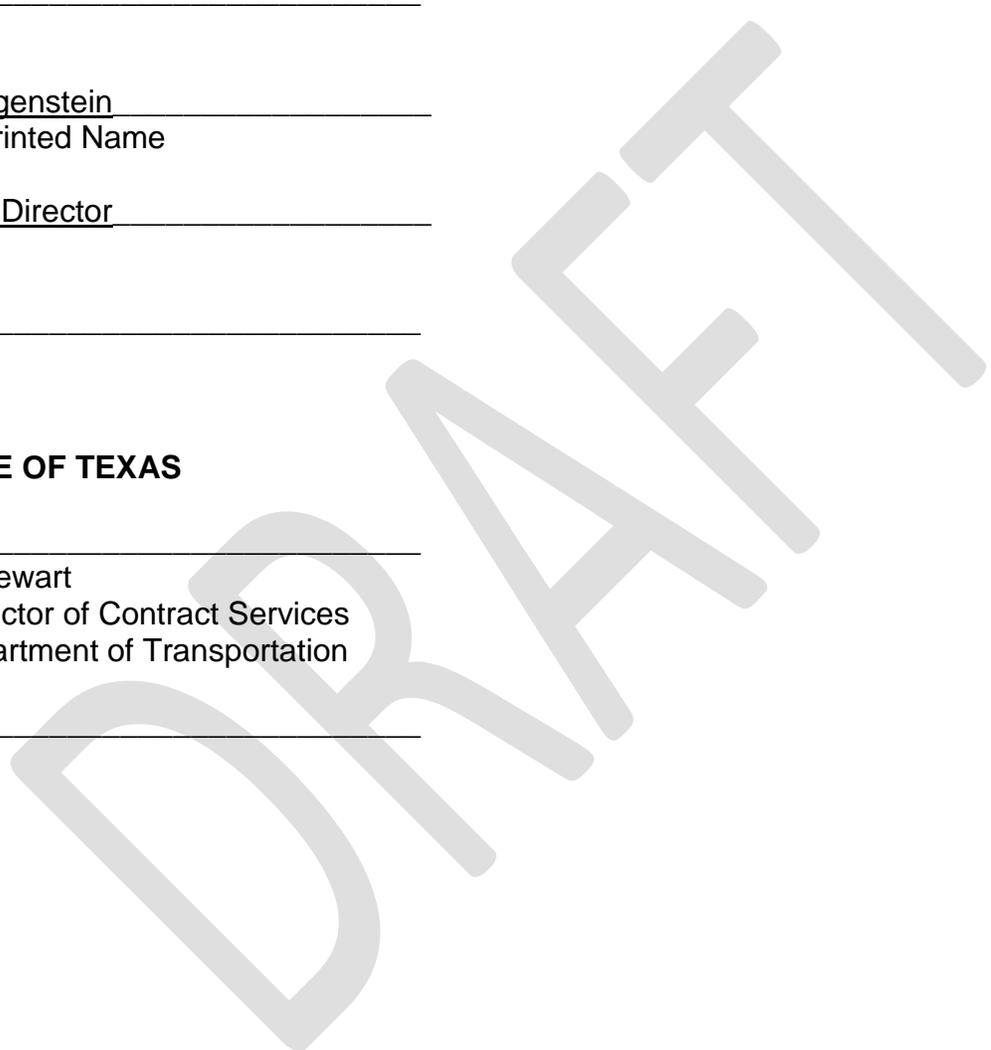
Title

Date

THE STATE OF TEXAS

Kenneth Stewart
Interim Director of Contract Services
Texas Department of Transportation

Date



**ATTACHMENT C-1
PROJECT BUDGET**

Costs will be allocated based on 100% Federal funding until the Federal funding reaches the maximum obligated amount. The Local Government will then be responsible for 100% of the costs.

0151-09-036							
Description	Total Estimated Cost	Federal Participation		State Participation		Local Participation	
		%	Cost	%	Cost	%	Cost
Preliminary Engineering (by LG)	\$7,638,240	100%	\$7,638,240	0%	\$0	0%	\$0
Environmental Support (by LG)	\$87,360	100%	\$87,360	0%	\$0	0%	\$0
Utility (by LG)	\$1,692,000	100%	\$1,692,000	0%	\$0	0%	\$0
Right of Way (by LG)	\$292,800	100%	\$292,800	0%	\$0	0%	\$0
Subtotal	\$9,710,400	\$9,710,400		\$0		\$0	
Direct State Costs for PE	\$152,765	0%	\$0	100%	\$152,765	0%	\$0
Direct State Costs for ENV	\$76,382	0%	\$0	100%	\$76,382	0%	\$0
Direct State Costs for UTIL	\$76,382	0%	\$0	100%	\$76,382	0%	\$0
Direct State Costs for ROW	\$76,382	0%	\$0	100%	\$76,382	0%	\$0
Indirect State Costs (4.83%)	\$368,927	0%	\$0	100%	\$368,927	0%	\$0
Subtotal	\$750,839	0%		\$750,839		\$0	
TOTAL CSJ 0151-09-036	\$10,461,239	\$9,710,400		\$750,839		\$0	
0151-09-127							
Description	Total Estimated Cost	Federal Participation		State Participation		Local Participation	
		%	Cost	%	Cost	%	Cost
Preliminary Engineering (by LG)	\$5,092,160	100%	\$5,092,160	0%	\$0	0%	\$0
Environmental Support (by LG)	\$58,240	100%	\$58,240	0%	\$0	0%	\$0
Utility (by LG)	\$1,128,000	100%	\$1,128,000	0%	\$0	0%	\$0
Right of Way (by LG)	\$195,200	100%	\$195,200	0%	\$0	0%	\$0
Subtotal	\$6,473,600	\$6,473,600		\$0		\$0	
Direct State Costs for PE	\$101,843	0%	\$0	100%	\$101,843	0%	\$0
Direct State Costs for ENV	\$50,922	0%	\$0	100%	\$50,922	0%	\$0
Direct State Costs for UTIL	\$50,922	0%	\$0	100%	\$50,922	0%	\$0
Direct State Costs for ROW	\$50,922	0%	\$0	100%	\$50,922	0%	\$0
Indirect State Costs (4.83%)	\$245,951	0%	\$0	100%	\$245,951	0%	\$0
Subtotal	\$500,559	\$0		\$500,559		\$0	
TOTAL CSJ 0151-09-127	\$6,974,159	\$6,473,600		\$500,559		\$0	

CSJ # 0151-09-036, 0151-09-127,
0265-01-080
District # 14 - AUS
Code Chart 64 #60432
Project: US 183S Bergstrom
Expressway
Federal Highway Administration
CFDA # 20.205
Not Research and Development

0265-01-080							
Description	Total Estimated Cost	Federal Participation		State Participation		Local Participation	
		%	Cost	%	Cost	%	Cost
Preliminary Engineering (by LG)	\$3,182,600	100%	\$3,182,600	0%	\$0	0%	\$0
Environmental Support (by LG)	\$36,400	100%	\$36,400	0%	\$0	0%	\$0
Utility (by LG)	\$705,000	100%	\$705,000	0%	\$0	0%	\$0
Right of Way (by LG)	\$122,000	100%	\$122,000	0%	\$0	0%	\$0
Subtotal	\$4,046,000		\$4,046,000		\$0		\$0
Direct State Costs	\$63,652	0%	\$0	100%	\$63,652	0%	\$0
Direct State Costs for ENV	\$31,826	0%	\$0	100%	\$31,826	0%	\$0
Direct State Costs for UTIL	\$31,826	0%	\$0	100%	\$31,826	0%	\$0
Direct State Costs for ROW	\$31,826	0%	\$0	100%	\$31,826	0%	\$0
Indirect State Costs (4.83%)	\$153,720	0%	\$0	100%	\$153,720	0%	\$0
Subtotal	\$312,850		\$0		\$312,850		\$0
TOTAL CSJ 0151-09-036	\$4,358,850		\$4,046,000		\$312,850		\$0
TOTAL CSJs 0151-09-036, 0151-09-127, 0151-09-036	\$21,794,248		\$20,230,000		\$1,564,248		\$0

Initial Payment by the Local Government to the State: \$0
Payment by the Local Government to the State before construction: \$0
Estimated total payment by the Local Government to the State; \$0
This is an estimate. The final amount of Local Government participation will be based on actual costs.



**CENTRAL TEXAS
Regional Mobility Authority**

AGENDA ITEM #9 SUMMARY

Authorize the Executive Director to execute an advance funding agreement with the Texas Department of Transportation for the Oak Hill Parkway Project.

Strategic Plan Relevance:	Regional Mobility
Department:	Engineering
Associated Costs:	\$5,930,000
Funding Source:	TxDOT Advance Funding Agreement
Board Action Required:	Yes

Description of Matter: This Advance Funding Agreement for \$5,930,000 in STP MM funds to the Mobility Authority will cover the costs associated with preliminary project development of the Oak Hill Parkway Project. These efforts will include, but not be limited to, Project management, administrative tasks, right of way, utilities, community outreach, and preliminary and final design.

Reference documentation: Draft Resolution
Advance Funding Agreement

Contact for further information: Wesley M. Burford, P.E., Director of Engineering

**GENERAL MEETING OF THE BOARD OF DIRECTORS
OF THE
CENTRAL TEXAS REGIONAL MOBILITY AUTHORITY**

RESOLUTION NO. 14-___

**AUTHORIZING THE EXECUTIVE DIRECTOR TO EXECUTE AN ADVANCE
FUNDING AGREEMENT WITH THE TEXAS DEPARTMENT OF
TRANSPORTATION FOR THE OAK HILL PARKWAY PROJECT.**

WHEREAS, the Central Texas Regional Mobility Authority (“Mobility Authority”) is empowered to study and develop potential transportation projects; and

WHEREAS, the Mobility Authority has proposed development of a project on U.S. 290 from west of Scenic Brook to Joe Tanner Lane, commonly referred to as the Oak Hill Parkway Project (the “Project”); and

WHEREAS, financial assistance from the Texas Department of Transportation, specifically an advance funding agreement, is available to cover preliminary project development activities for the Project; and

WHEREAS, the Executive Director recommends approval of the proposed advanced funding agreement for the Project in the form or substantially in the form attached as Exhibit 1 (the “AFA”).

NOW THEREFORE, BE IT RESOLVED, that the Board of Directors approves the AFA, and authorizes the Executive Director to finalize and execute the AFA in the form or substantially in the form attached as Exhibit 1.

Adopted by the Board of Directors of the Central Texas Regional Mobility Authority on the 30th day of July, 2014.

Submitted and reviewed by:

Approved:

Andrew Martin
General Counsel for the Central
Texas Regional Mobility Authority

Ray A. Wilkerson
Chairman, Board of Directors
Resolution Number: 14-___
Date Passed: 07/30/14

Exhibit 1

Advance Funding Agreement for the Oak Hill Parkway

[on the following 14 pages]

STATE OF TEXAS §
COUNTY OF TRAVIS §

**ADVANCE FUNDING AGREEMENT
For A
Metropolitan Mobility and Rehabilitation Project
On-System**

THIS AGREEMENT is made by and between the State of Texas, acting by and through the Texas Department of Transportation called the "State", and the Central Texas Regional Mobility Authority, acting by and through its duly authorized officials, called the "Local Government."

WITNESSETH

WHEREAS, federal law establishes federally funded programs for transportation improvements to implement its public purposes; and

WHEREAS, the Texas Transportation Code, Sections 201.103 and 222.052 establish that the State shall design, construct and operate a system of highways in cooperation with local governments; and

WHEREAS, federal and state laws require local governments to meet certain contract standards relating to the management and administration of State and federal funds; and

WHEREAS, the Texas Transportation Commission passed Minute Order Number 113948, authorizing the State to undertake and complete a highway improvement generally described as construct main lanes (Toll) and frontage roads called the "Project"; and,

WHEREAS, House Bill 2585 of the 83rd regular session authorizes reimbursement of utilities for relocation of utility facilities following improvement or construction of certain tolled highways; and,

WHEREAS, the Governing Body of the Local Government has approved entering into this agreement by resolution or ordinance dated _____, 20__, which is attached to and made a part of this agreement as Attachment "A" for the improvement covered by this agreement. A map showing the Project location appears in Attachment "B," which is attached to and made a part of this agreement.

NOW, THEREFORE, in consideration of the premises and of the mutual covenants and agreements of the parties, to be by them respectively kept and performed as set forth in this agreement, it is agreed as follows:

AGREEMENT

1. Period of the Agreement

This agreement becomes effective when signed by the last party whose signing makes the agreement fully executed. This agreement shall remain in effect until the Project is completed or unless terminated as provided below.

2. Scope of Work

The scope of work is the development of U.S. 290 from west of Scenic Brook to Joe Tanner Lane by the Central Texas Regional Mobility Authority as shown on Attachment "B". Work will include environmental support, preliminary engineering, utility coordination and project coordination.

3. Local Project Sources and Uses of Funds

- A. The total estimated cost of the Project is shown in the Project Budget – Attachment "C", which is attached to and made a part of this agreement. The expected cash contributions from the Federal or State government, the Local Governments, or other parties is shown in Attachment "C". The State will pay for only those project costs that have been approved by the Texas Transportation Commission. The State and the Federal Government will not reimburse the Local Government for any work performed before the federal spending authority is formally obligated to the Project by the Federal Highway Administration. After federal funds have been obligated, the State will send to the Local Government a copy of the formal documentation showing the obligation of funds including federal award information. The Local Government is responsible for 100% of the cost of any work performed under its direction or control before the federal spending authority is formally obligated.
- B. If the Local Government will perform any work under this contract for which reimbursement will be provided by or through the State, the Local Government must complete training before federal spending authority is obligated. Training is complete when at least one individual who is working actively and directly on the Project successfully completes and receives a certificate for the course entitled *Local Government Project Procedures Qualification for the Texas Department of Transportation*. The Local Government shall provide the certificate of qualification to the State. The individual who receives the training certificate may be an employee of the Local Government or an employee of a firm that has been contracted by the Local Government to perform oversight of the Project. The State in its discretion may deny reimbursement if the Local Government has not designated a qualified individual to oversee the Project.
- C. This Project cost estimate shows how necessary resources for completing the Project will be provided by major cost categories. These categories may include but are not limited to: (1) costs of real property; (2) costs of utility work; (3) costs of environmental assessment and remediation; (4) cost of preliminary engineering and design; (5) cost of construction and construction management; and (6) any other local project costs.
- D. The State will be responsible for securing the Federal and State share of the funding required for the development and construction of the local Project. If the Local Government is due funds for expenses incurred, these funds will be reimbursed to the Local Government on a cost basis.

- E. The Local Government will be responsible for all non-federal or non-state participation costs associated with the Project, including any overruns in excess of the approved local project budget unless otherwise provided for in this agreement or approved otherwise in an amendment to this agreement.
- F. Prior to the performance of any engineering review work by the State, the Local Government will pay to the State the amount specified in Attachment C. At a minimum, this amount shall equal the Local Government's funding share for the estimated cost of preliminary engineering for the Project. At least sixty (60) days prior to the date set for receipt of the construction bids, the Local Government shall remit its remaining financial share for the State's estimated construction oversight and construction cost.
- G. In the event that the State determines that additional funding by the Local Government is required at any time during the Project, the State will notify the Local Government in writing. The Local Government shall make payment to the State within thirty (30) days from receipt of the State's written notification. **Not Applicable**
- H. Whenever funds are paid by the Local Government to the State under this agreement, the Local Government shall remit a check or warrant made payable to the "Texas Department of Transportation Trust Fund." The check or warrant shall be deposited by the State in an escrow account to be managed by the State. Funds in the escrow account may only be applied to the State Project.
- I. Upon completion of the Project, the State will perform an audit of the Project costs. Any funds due by the Local Government, the State, or the Federal government will be promptly paid by the owing party. If, after final Project accounting, excess funds remain in the escrow account, those funds may be applied by the State to the Local Government's contractual obligations to the State under another advance funding agreement with approval by appropriate personnel of the Local Government.
- J. The State will not pay interest on any funds provided by the Local Government.
- K. If a waiver has been granted, the State will not charge the Local Government for the indirect costs the State incurs on the local Project, unless this agreement is terminated at the request of the Local Government prior to completion of the Project.
- L. If the Project has been approved for a "fixed price" or an "incremental payment" non-standard funding or payment arrangement under 43 TAC §15.52, the budget in Attachment C will clearly state the amount of the fixed price or the incremental payment schedule.
- M. If the Local government is an Economically Disadvantaged County and if the State has approved adjustments to the standard financing arrangement, this agreement reflects those adjustments.
- N. The state auditor may conduct an audit or investigation of any entity receiving funds from the State directly under this contract or indirectly through a subcontract under this contract. Acceptance of funds directly under this contract or indirectly through a subcontract under this contract acts as acceptance of the authority of the state auditor, under the direction of the legislative audit committee, to conduct an audit or investigation in connection with those funds. An entity that is the subject of an audit or investigation must provide the state auditor with access to any information the state auditor considers relevant to the investigation or audit.

- O. Payment under this contract beyond the end of the current fiscal biennium is subject to availability of appropriated funds. If funds are not appropriated, this contract shall be terminated immediately with no liability to either party.
- P. The Local Government is authorized to submit requests for reimbursement by submitting the original of an itemized invoice in a form and containing all items required by the State no more frequently than monthly, and no later than ninety (90) days after costs are incurred. If the Local Government submits invoices more than ninety (90) days after the costs are incurred, and if federal funding is reduced as a result, the State shall have no responsibility to reimburse the Local Government for those costs.
- Q. The State will not execute the contract for the construction of the Project until the required funding has been made available by the Local Government in accordance with this agreement. **Not Applicable**

4. Termination of this Agreement

This agreement shall remain in effect until the project is completed and accepted by all parties, unless:

- A. The agreement is terminated in writing with the mutual consent of the parties;
- B. The agreement is terminated by one party because of a breach, in which case any cost incurred because of the breach shall be paid by the breaching party;
- C. The Local Government elects not to provide funding after the completion of preliminary engineering, specifications, and estimates (PS&E) and the Project does not proceed because of insufficient funds, in which case the Local Government agrees to reimburse the State for its reasonable actual costs incurred during the Project; or **Not Applicable**
- D. The Project is inactive for thirty-six (36) months or longer and no expenditures have been charged against federal funds, in which case the State may in its discretion terminate this agreement.

5. Amendments

Amendments to this agreement due to changes in the character of the work, terms of the agreement, or responsibilities of the parties relating to the Project may be enacted through a mutually agreed upon, written amendment.

6. Remedies

This agreement shall not be considered as specifying the exclusive remedy for any agreement default, but all remedies existing at law and in equity may be availed of by either party to this agreement and shall be cumulative.

7. Utilities

The Local Government shall be responsible for the adjustment, removal, or relocation of utility facilities in accordance with applicable State laws, regulations, rules, policies, and procedures, including any cost to the State of a delay resulting from the Local Government's failure to ensure that utility facilities are adjusted, removed, or relocated before the scheduled beginning of construction. The Local Government must obtain advance approval for any variance from established procedures.

8. Environmental Assessment and Mitigation

Development of a transportation project must comply with the National Environmental Policy Act and the National Historic Preservation Act of 1966, which require environmental clearance of federal-aid projects.

- A. The State is responsible for the identification and assessment of any environmental problems associated with the development of a local project governed by this agreement.
- B. The State is responsible for the cost of any environmental problem's mitigation and remediation.
- C. The State is responsible for providing any public meetings or public hearings required for development of the environmental assessment. Public hearings will not be held prior to the approval of project schematic.
- D. The State is responsible for the preparation of the NEPA documents required for the environmental clearance of this Project.
- E. Before the advertisement for bids, the Local Government shall provide to the State written documentation from the appropriate regulatory agency or agencies that all environmental clearances have been obtained. **Not Applicable**

9. Compliance with Texas Accessibility Standards and ADA

All parties to this agreement shall ensure that the plans for and the construction of all projects subject to this agreement are in compliance with the Texas Accessibility Standards (TAS) issued by the Texas Department of Licensing and Regulation, under the Architectural Barriers Act, Article 9102, Texas Civil Statutes. The TAS establishes minimum accessibility requirements to be consistent with minimum accessibility requirements of the Americans with Disabilities Act (P.L. 101-336) (ADA).

10. Architectural and Engineering Services

The Local Government has responsibility for the performance of architectural and engineering services. The engineering plans shall be developed in accordance with the applicable *State's Standard Specifications for Construction and Maintenance of Highways, Streets and Bridges* and the special specifications and special provisions related to it. For projects on the state highway system, the design shall, at a minimum conform to applicable State manuals. For projects not on the state highway system, the design shall, at a minimum, conform to applicable *American Association of State Highway and Transportation Officials* design standards. In procuring professional services, the parties to this agreement must comply with federal requirements cited in 23 CFR Part 172 if the project is federally funded and with Texas Government Code 2254, Subchapter A, in all cases. Professional contracts for federally funded projects must conform to federal requirements, specifically including the provision for participation by Disadvantaged Business Enterprises (DBEs), ADA, and environmental matters.

11. Construction Responsibilities Not Applicable

- A. The State shall advertise for construction bids, issue bid proposals, receive and tabulate the bids, and award and administer the contract for construction of the Project. Administration of the contract includes the responsibility for construction engineering and for issuance of any change orders, supplemental agreements, amendments, or additional work orders that may become necessary subsequent to the award of the construction

contract. In order to ensure federal funding eligibility, projects must be authorized by the State prior to advertising for construction.

- B.** The State will use its approved contract letting and award procedures to let and award the construction contract.
- C.** Prior to their execution, the Local Government will be given the opportunity to review contract change orders that will result in an increase in cost to the Local Government.
- D.** Upon completion of the Project, the party constructing the Project will issue and sign a "Notification of Completion" acknowledging the Project's construction completion.
- E.** For federally funded contracts, the parties to this agreement will comply with federal construction requirements cited in 23 CFR Part 635 and with requirements cited in 23 CFR Part 633, and shall include the latest version of Form "FHWA-1273" in the contract bidding documents. If force account work will be performed, a finding of cost effectiveness shall be made in compliance with 23 CFR 635, Subpart B.

12. Project Maintenance

The Local Government shall be responsible for maintenance of locally owned roads after completion of the work and the State shall be responsible for maintenance of state highway system after completion of the work if the work was on the state highway system, unless otherwise provided for in existing maintenance agreements with the Local Government.

13. Right of Way and Real Property Not Applicable

The State is responsible for the provision and acquisition of any needed right of way or real property.

14. Notices

All notices to either party shall be delivered personally or sent by certified or U.S. mail, postage prepaid, addressed to that party at the following address:

Local Government:	State:
Central Texas RMA Attn: Executive Director 3300 N IH-35, Suite 300 Austin, Texas 78705	Director of Contract Services Office Texas Department of Transportation 125 E. 11 th Street Austin, Texas 78701

All notices shall be deemed given on the date delivered in person or deposited in the mail, unless otherwise provided by this agreement. Either party may change the above address by sending written notice of the change to the other party. Either party may request in writing that notices shall be delivered personally or by certified U.S. mail, and that request shall be carried out by the other party.

15. Legal Construction

If one or more of the provisions contained in this agreement shall for any reason be held invalid, illegal, or unenforceable in any respect, such invalidity, illegality, or unenforceability shall not affect any other provisions and this agreement shall be construed as if it did not contain the invalid, illegal, or unenforceable provision.

16. Responsibilities of the Parties

The State and the Local Government agree that neither party is an agent, servant, or employee of the other party and each party agrees it is responsible for its individual acts and deeds as well as the acts and deeds of its contractors, employees, representatives, and agents.

17. Ownership of Documents

Upon completion or termination of this agreement, all documents prepared by the State shall remain the property of the State. All data prepared under this agreement shall be made available to the State without restriction or limitation on their further use. All documents produced or approved or otherwise created by the Local Government shall be transmitted to the State in the form of photocopy reproduction on a monthly basis as required by the State. The originals shall remain the property of the Local Government. At the request of the State, the Local Government shall submit any information required by the State in the format directed by the State.

18. Compliance with Laws

The parties shall comply with all federal, state, and local laws, statutes, ordinances, rules and regulations, and the orders and decrees of any courts or administrative bodies or tribunals in any manner affecting the performance of this agreement. When required, the Local Government shall furnish the State with satisfactory proof of this compliance.

19. Sole Agreement

This agreement constitutes the sole and only agreement between the parties and supersedes any prior understandings or written or oral agreements respecting the agreement's subject matter.

20. Cost Principles

In order to be reimbursed with federal funds, the parties shall comply with the Cost Principles established in OMB Circular A-87 that specify that all reimbursed costs are allowable, reasonable, and allocable to the Project.

21. Procurement and Property Management Standards

The parties shall adhere to the procurement standards established in Title 49 CFR §18.36 and with the property management standard established in Title 49 CFR §18.32.

22. Inspection of Books and Records

The parties to this agreement shall maintain all books, documents, papers, accounting records, and other documentation relating to costs incurred under this agreement and shall make such materials available to the State, the Local Government, and, if federally funded, the Federal Highway Administration (FHWA), and the U.S. Office of the Inspector General, or their duly

authorized representatives for review and inspection at its office during the contract period and for four (4) years from the date of completion of work defined under this contract or until any impending litigation, or claims are resolved. Additionally, the State, the Local Government, and the FHWA and their duly authorized representatives shall have access to all the governmental records that are directly applicable to this agreement for the purpose of making audits, examinations, excerpts, and transcriptions.

23. Civil Rights Compliance

The Local Government shall comply with the regulations of the United States Department of Transportation as they relate to non-discrimination (49 CFR Part 21 and 23 CFR Part 200), and Executive Order 11246 titled "Equal Employment Opportunity," as amended by Executive Order 11375 and supplemented in the Department of Labor Regulations (41 CFR Part 60).

24. Disadvantaged Business Enterprise (DBE) Program Requirements

- A. The parties shall comply with the Disadvantaged Business Enterprise Program requirements established in 49 CFR Part 26.
- B. The Local Government shall adopt, in its totality, the State's federally approved DBE program.
- C. The Local Government shall set an appropriate DBE goal consistent with the State's DBE guidelines and in consideration of the local market, project size, and nature of the goods or services to be acquired. The Local Government shall have final decision-making authority regarding the DBE goal and shall be responsible for documenting its actions.
- D. The Local Government shall follow all other parts of the State's DBE program referenced in TxDOT Form 2395, Memorandum of Understanding Regarding the Adoption of the Texas Department of Transportation's Federally-Approved Disadvantaged Business Enterprise by Entity, and attachments found at web address http://ftp.dot.state.tx.us/pub/txdot-info/bop/dbe/mou/mou_attachments.pdf.
- E. The Local Government shall not discriminate on the basis of race, color, national origin, or sex in the award and performance of any U.S. Department of Transportation (DOT)-assisted contract or in the administration of its DBE program or the requirements of 49 CFR Part 26. The Local Government shall take all necessary and reasonable steps under 49 CFR Part 26 to ensure non-discrimination in award and administration of DOT-assisted contracts. The State's DBE program, as required by 49 CFR Part 26 and as approved by DOT, is incorporated by reference in this agreement. Implementation of this program is a legal obligation and failure to carry out its terms shall be treated as a violation of this agreement. Upon notification to the Local Government of its failure to carry out its approved program, the State may impose sanctions as provided for under 49 CFR Part 26 and may, in appropriate cases, refer the matter for enforcement under 18 U.S.C. 1001 and the Program Fraud Civil Remedies Act of 1986 (31 U.S.C. 3801 et seq.).
- F. Each contract the Local Government signs with a contractor (and each subcontract the prime contractor signs with a sub-contractor) must include the following assurance: *The contractor, sub-recipient, or sub-contractor shall not discriminate on the basis of race, color, national origin, or sex in the performance of this contract. The contractor shall carry out applicable requirements of 49 CFR Part 26 in the award and administration of DOT-assisted contracts. Failure by the contractor to carry out these requirements is a material*

breach of this agreement, which may result in the termination of this agreement or such other remedy as the recipient deems appropriate.

25. Debarment Certifications

The parties are prohibited from making any award at any tier to any party that is debarred or suspended or otherwise excluded from or ineligible for participation in Federal Assistance Programs under Executive Order 12549, "Debarment and Suspension." By executing this agreement, the Local Government certifies that it and its principals are not currently debarred, suspended, or otherwise excluded from or ineligible for participation in Federal Assistance Programs under Executive Order 12549 and further certifies that it will not do business with any party, to include principals, that is currently debarred, suspended, or otherwise excluded from or ineligible for participation in Federal Assistance Programs under Executive Order 12549. The parties to this contract shall require any party to a subcontract or purchase order awarded under this contract to certify its eligibility to receive federal funds and, when requested by the State, to furnish a copy of the certification.

26. Lobbying Certification

In executing this agreement, each signatory certifies to the best of that signatory's knowledge and belief, that:

- A.** No federal appropriated funds have been paid or will be paid by or on behalf of the parties to any person for influencing or attempting to influence an officer or employee of any federal agency, a Member of Congress, an officer or employee of Congress, or an employee of a Member of Congress in connection with the awarding of any federal contract, the making of any federal grant, the making of any federal loan, the entering into of any cooperative agreement, and the extension, continuation, renewal, amendment, or modification of any federal contract, grant, loan, or cooperative agreement.
- B.** If any funds other than federal appropriated funds have been paid or will be paid to any person for influencing or attempting to influence an officer or employee of any agency, a Member of Congress, an officer or employee of Congress, or an employee of a Member of Congress in connection with federal contracts, grants, loans, or cooperative agreements, the signatory for the Local Government shall complete and submit the Federal Standard Form-LLL, "Disclosure Form to Report Lobbying," in accordance with its instructions.
- C.** The parties shall require that the language of this certification shall be included in the award documents for all sub-awards at all tiers (including subcontracts, subgrants, and contracts under grants, loans, and cooperative agreements) and all sub-recipients shall certify and disclose accordingly. Submission of this certification is a prerequisite for making or entering into this transaction imposed by Title 31 U.S.C. §1352. Any person who fails to file the required certification shall be subject to a civil penalty of not less than \$10,000 and not more than \$100,000 for each such failure.

27. Insurance

If this agreement authorizes the Local Government or its contractor to perform any work on State right of way, before beginning work the entity performing the work shall provide the State with a fully executed copy of the State's Form 1560 Certificate of Insurance verifying the existence of coverage in the amounts and types specified on the Certificate of Insurance for all

persons and entities working on State right of way. This coverage shall be maintained until all work on the State right of way is complete. If coverage is not maintained, all work on State right of way shall cease immediately, and the State may recover damages and all costs of completing the work.

28. Federal Funding Accountability and Transparency Act Requirements

- A.** Any recipient of funds under this agreement agrees to comply with the Federal Funding Accountability and Transparency Act (FFATA) and implementing regulations at 2 CFR Part 170, including Appendix A. This agreement is subject to the following award terms:
<http://www.gpo.gov/fdsys/pkg/FR-2010-09-14/pdf/2010-22705.pdf> and
<http://www.gpo.gov/fdsys/pkg/FR-2010-09-14/pdf/2010-22706.pdf>.
- B.** The Local Government agrees that it shall:
1. Obtain and provide to the State a System for Award Management (SAM) number (Federal Acquisition Regulation, Part 4, Sub-part 4.11) if this award provides more than \$25,000 in Federal funding. The SAM number may be obtained by visiting the SAM website whose address is: <https://www.sam.gov/portal/public/SAM/>
 2. Obtain and provide to the State a Data Universal Numbering System (DUNS) number, a unique nine-character number that allows Federal government to track the distribution of federal money. The DUNS may be requested free of charge for all businesses and entities required to do so by visiting the Dun & Bradstreet (D&B) on-line registration website <http://fedgov.dnb.com/webform>; and
 3. Report the total compensation and names of its top five (5) executives to the State if:
 - i. More than 80% of annual gross revenues are from the Federal government, and those revenues are greater than \$25,000,000; and
 - ii. The compensation information is not already available through reporting to the U.S. Securities and Exchange Commission.

29. Single Audit Report

- A.** The parties shall comply with the requirements of the Single Audit Act of 1984, P.L. 98-502, ensuring that the single audit report includes the coverage stipulated in OMB Circular A-133.
- B.** If threshold expenditures of \$750,000 or more are met during the Local Government's fiscal year, the Local Government must submit a Single Audit Report and Management Letter (if applicable) to TxDOT's Audit Office, 125 E. 11th Street, Austin, TX 78701 or contact TxDOT's Audit Office at <http://www.txdot.gov/inside-txdot/office/audit/contact.html>.
- C.** If expenditures are less than \$750,000 during the Local Government's fiscal year, the Local Government must submit a statement to TxDOT's Audit Office as follows: "We did not meet the \$750,000 expenditure threshold and therefore, are not required to have a single audit performed for FY _____."
- D.** For each year the project remains open for federal funding expenditures, the Local Government will be responsible for filing a report or statement as described above. The required annual filing shall extend throughout the life of the agreement, unless otherwise amended or the project has been formally closed out and no charges have been incurred within the current fiscal year.

30. Signatory Warranty

Each signatory warrants that the signatory has necessary authority to execute this agreement on behalf of the entity represented.

THIS AGREEMENT IS EXECUTED by the State and the Local Government in duplicate.

THE LOCAL GOVERNMENT

Signature

Mike Heiligenstein

Typed or Printed Name

Executive Director

Title

Date

THE STATE OF TEXAS

Kenneth Stewart
Interim Director of Contract Services
Texas Department of Transportation

Date

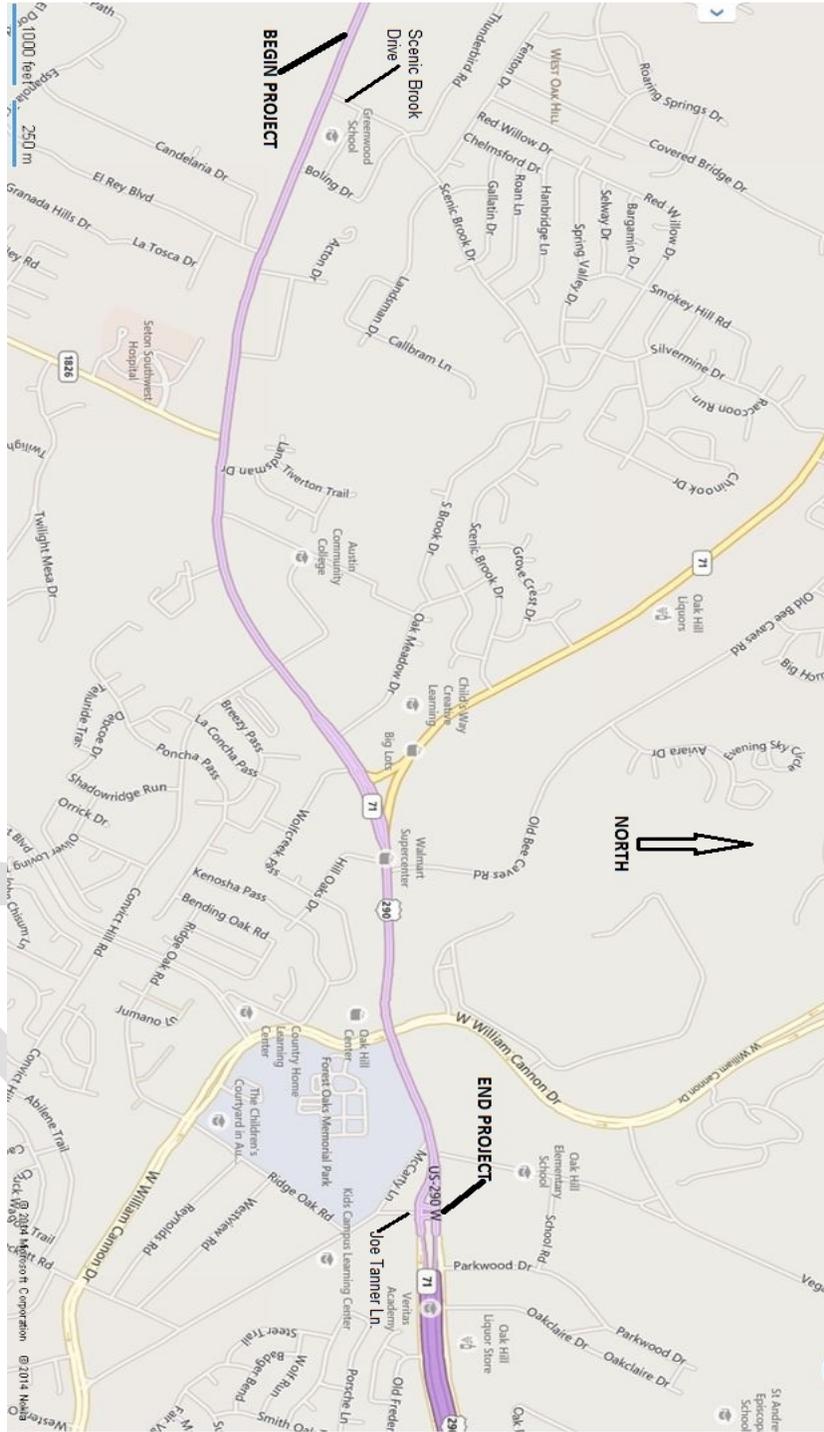
CSJ # 0113-08-060
District # 14-AUS
Code Chart 64 #60432
Project: U.S. 290, Scenic Brook to Joe Tanner
Federal Highway Administration
CFDA # 20.205
Not Research and Development

**ATTACHMENT A
RESOLUTION OR ORDINANCE**

DRAFT

CSJ # 0113-08-060
District # 14-AUS
Code Chart 64 #60432
Project: U.S. 290, Scenic Brook to Joe Tanner
Federal Highway Administration
CFDA # 20.205
Not Research and Development

ATTACHMENT B LOCATION MAP SHOWING PROJECT



ATTACHMENT C PROJECT BUDGET

Costs will be allocated based on 80% Federal funding and 20% State Government funding until the federal funding reaches the maximum obligated amount. The Local Government will then be responsible for 100% of the costs.

Description	Total Estimated Cost	Federal Participation		State Participation		Local Participation	
		%	Cost	%	Cost	%	Cost
Preliminary Engineering (by LG)	\$5,830,000	80%	\$4,664,000	20%	\$1,166,000	0%	\$0
Environmental Support (by LG)	\$50,000	80%	\$40,000	20%	\$10,000	0%	\$0
Utility (by LG)	\$50,000	80%	\$40,000	20%	\$10,000	0%	\$0
SUBTOTAL	\$5,930,000	\$4,744,000		\$1,186,000		\$0	
Engineering Direct State Costs	\$116,600	0%	\$0	100%	\$116,600	0%	\$0
Environmental Direct State Costs	\$58,300	0%	\$0	100%	\$58,300	0%	\$0
Utility Direct State Costs	\$29,150	0%	\$0	100%	\$29,150	0%	\$0
Indirect State Costs (4.83%)	\$281,589	0%	\$0	100%	\$281,589	0%	\$0
SUBTOTAL	\$485,639	\$0		\$485,639		\$0	
TOTAL	\$6,415,639	\$4,744,000		\$1,671,639		\$0	

Initial payment by the Local Government to the State: \$0

Payment by the Local Government to the State before construction: \$0

Estimated total payment by the Local Government to the State \$0

This is an estimate. The final amount of Local Government participation will be based on actual costs.



**CENTRAL TEXAS
Regional Mobility Authority**

AGENDA ITEM #10 SUMMARY

Approve a work authorization with Atkins North America, Inc., to provide general engineering consultant services for the SH 71 Express Project.

Strategic Plan Relevance:	Regional Mobility
Department:	Engineering
Associated Costs:	\$1,470,000
Funding Source:	General Fund, Reimbursed per Project Agreement with TxDOT
Board Action Required:	Yes

Description of Matter: Atkins North America, Inc. will provide General Engineering Consultant services related to project activities required to assist the Mobility Authority in the development of the SH 71 Express Project. These efforts will include, but not be limited to, project management, administrative tasks, and program oversight including: coordination with TxDOT, Consultants, Resource Agencies, Design/Construction Oversight, Toll Systems Integrator Oversight, Toll Lane Maintenance and Operations, Community Outreach and additional activities as specifically requested by the Authority.

Reference documentation: Proposed Work Authorization #11

Contact for further information: Wesley M. Burford, P.E., Director of Engineering

**GENERAL MEETING OF THE BOARD OF DIRECTORS
OF THE
CENTRAL TEXAS REGIONAL MOBILITY AUTHORITY**

RESOLUTION NO. 14-___

**APPROVING A WORK AUTHORIZATION WITH ATKINS NORTH AMERICA, INC.,
TO PROVIDE GENERAL ENGINEERING CONSULTANT SERVICES FOR THE SH 71
EXPRESS PROJECT.**

WHEREAS, Atkins North America, Inc., (“Atkins”) serves as a general engineering consultant to the Mobility Authority under the Agreement for General Consulting Civil Engineering Services effective January 1, 2010 (the “GEC Agreement”); and

WHEREAS, the Executive Director and Atkins have discussed and agreed to a proposed work authorization for Atkins to provide general engineering consultant services for the SH 71 Express Project; and

WHEREAS, the Executive Director recommends approval of the proposed work authorization attached as Exhibit 1.

NOW THEREFORE, BE IT RESOLVED that the proposed work authorization is hereby approved; and

BE IT FURTHER RESOLVED that the Executive Director may finalize and execute for the Mobility Authority the proposed work authorization in the form or substantially the same form as Exhibit 1.

Adopted by the Board of Directors of the Central Texas Regional Mobility Authority on the 30th day of July, 2014.

Submitted and reviewed by:

Approved:

Andrew Martin
General Counsel for the Central
Texas Regional Mobility Authority

Ray A. Wilkerson
Chairman, Board of Directors
Resolution Number: 14-___
Date Passed: 7/30/2014

EXHIBIT 1 TO RESOLUTION 14-
PROPOSED WORK AUTHORIZATION

[on the following 15 pages]

EXHIBIT D
WORK AUTHORIZATION

Work Authorization No. 11

This Work Authorization is made as of this 1st day of June, 2014, under the terms and conditions established in the AGREEMENT FOR GENERAL CONSULTING ENGINEERING SERVICES, dated as of January 4th, 2010 (the Agreement), between the **Central Texas Regional Mobility Authority** (Authority) and **Atkins North America, Inc.** (GEC). This Work Authorization is made for the following purpose, consistent with the services defined in the Agreement:

*State Highway 71 Toll Lanes
Oversight Services*

Section A. - Scope of Services

A.1. GEC shall perform the following Services:

Please reference Attachment A – Services to be Provided by the GEC

A.2. The following Services are not included in this Work Authorization, but shall be provided as Additional Services if authorized or confirmed in writing by the Authority.

Not applicable.

A.3. In conjunction with the performance of the foregoing Services, GEC shall provide the following submittals/deliverables (Documents) to the Authority:

Please reference Attachment A – Services to be Provided by the GEC

Section B. - Schedule

GEC shall perform the Services and deliver the related Documents (if any) according to the following schedule:

Services defined herein are expected to be substantially complete within thirty one (31) months from the date this Work Authorization becomes effective. This Work Authorization will not expire until all tasks associated with the Scope of Services are complete.

Section C. - Compensation

C.1. In return for the performance of the foregoing obligations, the Authority shall pay to the GEC the amount not to exceed \$1,470,000, based on Attachment B -Fee Estimate. Compensation shall be in accordance with the Agreement.

C.2. Compensation for Additional Services (if any) shall be paid by the Authority to

the GEC according to the terms of a future Work Authorization.

Section D. - Authority's Responsibilities

The Authority shall perform and/or provide the following in a timely manner so as not to delay the Services of the GEC. Unless otherwise provided in this Work Authorization, the Authority shall bear all costs incident to compliance with the following:

Please reference Attachment A – Services to be Provided by the GEC

Section E. - Other Provisions

The parties agree to the following provisions with respect to this specific Work Authorization:

Not applicable.

Except to the extent expressly modified herein, all terms and conditions of the Agreement shall continue in full force and effect.

Authority: Central Texas Regional Mobility Authority

GEC: Atkins North America, Inc.

By: Mike Heiligenstein

By: _____

Signature: _____

Signature: _____

Title: Executive Director

Title: _____

Date: _____

Date: _____

**CENTRAL TEXAS REGIONAL MOBILITY AUTHORITY
WORK AUTHORIZATION NO. 11**

Atkins

**ATTACHMENT A
SERVICES TO BE PROVIDED BY GEC**

INTRODUCTION

The work to be performed by the General Engineering Consultant (GEC) will include project management and oversight services of the toll systems design and implementation and technical support and oversight services as required in support of the Project Agreement (PA) between the Texas Department of Transportation (TxDOT) and the Central Texas Regional Mobility Authority (Mobility Authority) for the SH 71 Toll Lanes Project executed on December 11, 2013. The following tasks further define the work efforts to be performed, and shall include all applicable reporting requirements.

TASK 1. PROGRAM MANAGEMENT AND ADMINISTRATIVE SUPPORT [GL Code 13720]

Administrative and project management services will be provided as necessary for the successful completion of the SH 71 Toll Lanes Project. These services may include the development and implementation of any or all of the following services for the SH 71 Toll Lanes Project:

A. PROJECT ADMINISTRATION

The GEC will perform project administrative and coordination duties which include, but are not limited to, the following:

- Progress Reports
- Meetings
- Invoicing
- File Management
- Scheduling and Progress Tracking
- General Project Coordination
- Copies of all original counterparts or, if originals are unavailable, copies of all materials prepared by or for the Mobility Authority in connection with the Project, together with any and all other items or information in the possession of the Authority and useful to or necessary for TxDOT's completion of the Project, including any Authority context-sensitive design information or materials (4.b.3 of PA). These copies to be provided to TxDOT in compliance with the PA.

B. PROJECT REPORTING

The GEC will prepare project reports which include, but are not limited to, the following:

- Prepare and issue monthly reports to the Mobility Authority on the Project's status which will document any issues, delays encountered, and corrective actions as necessary. Reports will include, but are not limited to, the following:
 - Key milestones accomplished during the preceding month

- Meetings and key activities for the upcoming month
 - Issues requiring resolution
- Prepare a Quarterly Report for the Mobility Authority with an Executive Summary that provides a summary of the monthly reports and the overall Project progress
- Update Project Reports and Dashboard on a monthly basis

C. PROJECT SCHEDULE

The GEC will provide staff to coordinate the Project scheduling activities. Specific activities include, but are not limited to, the following:

- Evaluate and monitor the Developer's Project schedule (baseline and updates)
- Catalog, and archive Baseline Schedule and schedule revisions. Evaluate time impacts and identify any potential increase in Mobility Authority costs. Report conclusions and recommendations to the Mobility Authority
- Evaluate and monitor the toll System Integrator's schedule to facilitate implementation of an operable toll system at Substantial Completion of the managed lanes, subject to correction of punch list items (8.3.b of PA)

D. COORDINATION/MEETINGS

The GEC will support the Mobility Authority in coordination activities with TxDOT, FHWA, Developer, toll Systems Integrator, and others entities as identified and as directed by the Mobility Authority. As directed by the Mobility Authority, the GEC will attend meetings throughout the project development process which include, but are not limited to, the following:

- Attend post award pre-construction meeting
- Attend progress and any other regularly scheduled meetings (Monthly/weekly)
- Attend issue resolution meetings (As Needed)
- Attend meeting pertaining to the traffic control and maintenance of traffic that are held by the Contractor or interested parties
- Attend monthly Board of Directors meetings

TASK 2. DESIGN/CONSTRUCTION GEC OVERSIGHT [GL Code 13720]

The Design/Construction GEC oversight services include oversight and inspection of the tolled lanes portion of TxDOT's construction contract for the SH 71 Toll Lanes Project. The following tasks further outline the work efforts to be performed:

A. *MANAGEMENT AND GENERAL TECHNICAL SUPPORT*

The GEC will provide management assistance and technical support as required by the Mobility Authority toward the successful completion of the SH 71 Toll Lanes Project. Work efforts include, but are not limited to, the following:

- Provide review of potential change orders on the Project
- Review change order cost estimates prepared by the Contractor, evaluate Contractor claims for extension of time and provide comments to Mobility Authority
- Maintain log and retain all documents associated with potential change orders
- Review Project communications and correspondence
- Advise the Mobility Authority on matters of engineering related to interpretation of design details, construction techniques and procedures, specifications, standard construction details, and construction plans prepared by the Design Engineer(s)
- Seek clarifications, through TxDOT, from the Design Engineer(s) when necessary on the intent reflect in the design plans and specifications. The Engineer of Record will remain responsible for design related services
- Assist the Mobility Authority in meeting the following requirements of the Project Agreement with TxDOT:
 - Provide technical resource and support services to Mobility Authority personnel serving on the Design and Construction Technical Committee with oversight responsibility for project design and construction issues and directives to the contractor. Specific work efforts include, but are not limited to, the following:
 - Serve as a Mobility Authority representative of the Design and Construction Technical Committee if requested by the Mobility Authority (4.b.2 of PA)
 - Respond to inquiries, resolve issues, and participate in the review of design questions, requests for information, requests for changes, change orders, design and construction deviations, noncompliance reports and notices of design change and other technical questions from the design-build contractor and toll collection system integrator
 - Attend Design and Construction Technical Committee meetings as requested by Mobility Authority which will meet no less than monthly during the design and construction of the project. (10.a of the PA)
 - Provide support as requested by TxDOT to support TxDOTs efforts to obtain the environmental clearance, permits, approvals, and agreements necessary for the project (4.b.10 of PA)
 - Acquisition of ROW if required for the operation and maintenance of the tolled lanes (6.b. of PA)
 - Support the Mobility Authority in development of requests to TxDOT for modifications to the design and/or technical specifications

B. DOCUMENT REVIEWS

- Assist the Mobility Authority in meeting the following requirements of the Project Agreement with TxDOT:
 - The GEC will perform design reviews of the following plan submittals:
 - 30% plans
 - 60% plans
 - 90% plans
 - Release For Construction plans

- Early release construction plans
- Resolve issues and participation in the review of design questions, requests for information, requests for changes, change orders, design and construction deviations, noncompliance reports and notices of design change and other technical questions from the design-build contractor and toll collection system integrator
- Review procurement documents as necessary to determine compliance with the SH 71 Toll Lanes Project Agreement between TxDOT and the Mobility Authority (4.b.7 of PA)
- Review contract documents, technical requirements, and plans and specifications, and submit comments within 3 business days (4.b.5 of PA requires Mobility Authority to respond within 5 business days)
- Review and provide plans and technical specifications related to the infrastructure design as necessary to ensure accommodation of the Mobility Authority improvements and long-term maintenance of the Project (4.b.5 of PA)

C. CONSTRUCTION OVERSIGHT INSPECTIONS OF TOLLED LANES

The GEC will perform construction oversight inspection services which include, but are not limited to, the following:

- Perform and report construction inspections
- Perform construction materials testing services on an as-needed basis
- Review and report final documentation of construction quantities in support of the Contractor's draw requests
- Maintain diaries, logs, and records of the Contractor's progress
- Provide a digital photo log of the Project are during construction with heavy emphasis on areas with potential claim items/issues and on areas of real/potential public controversy

D. REQUESTS FOR INFORMATION (RFI) AND NON-CONFORMANCE REPORT PROCESSING

The GEC will review information and reports which include, but are not limited to, the following:

- Review and comment on Project RFIs
- Prepare reports on nonconforming work affecting the tolled lanes and submit to TxDOT for resolution
- Maintain a log of RFIs and NCRs

E. SHOP DRAWING/SUBMITTALS PROCESSING AND MANAGEMENT

The GEC will be responsible for reviewing drawings and general conformance which include, but are not limited to, the following:

- Review shop drawings, erection drawings, working drawings, samples, material and product certifications, and catalog cuts and brochure submittals for general conformance with the design

plans and specifications submitted by the Contractor. Check that the Contractor has received required approvals prior to construction

- Maintain a log of shop drawings

F. MOBILITY AUTHORITY CONSTRUCTION COORDINATION SUPPORT

The GEC will support the Mobility Authority in coordination and any interlocal agency agreements including exhibit preparation and supporting document preparation and assembly with the following agencies:

- Texas Department of Transportation (TxDOT)
- Federal Highway Administration (FHWA)
- City of Austin
- Travis County
- Local Municipalities and Municipal Utility Districts
- Other Agencies as identified and as directed by the Mobility Authority

G. NOTICE OF COMPLETION

The GEC will perform project close out activities which include, but are not limited to, the following:

- Coordinate with the Contractor and TxDOT in the generation of a punch list
- Inspect completion of punch list
- Verify that there are no outstanding claims related to the Contractor's work
- Provide a Notification of Completion to the Mobility Authority
- Assist the Mobility Authority in meeting the following requirements of the Project Agreement with TxDOT:
 - Participate in site inspections and development of punch list items following substantial completion of the work (8.b.1 of PA)
 - Participate in inspections at Final Acceptance of the Project (8.b.2 of PA)

TASK 3. TOLL SYSTEMS INTEGRATOR OVERSIGHT [GL Code 13720]

The GEC shall support the Mobility Authority as necessary with the development and implementation of the SH 71 Toll Lanes electronic toll collection system. These services include, but are not limited to, the following:

A. DESIGN AND CONSTRUCTION OVERSIGHT

- Coordinating the design and installation of the toll systems in a manner to avoid any delays in Project development and opening of the project (4.b.8 of PA)
- Provide GEC oversight of the Systems Integrator through design, construction, installation, testing, and commissioning

- Facilitate coordination efforts between the Systems Integrator and TxDOT's roadway construction contract

B. FINAL ACCEPTANCE

- Oversee final system acceptance and readiness testing for toll collection operations
- Coordinate development and submittal of Mobility Authority's written notification to TxDOT of substantial completion by the SI within 1 business day (8.b.4 of PA - Mobility Authority has to submit to TxDOT in 2 business days)
- Prior to deployment of toll collection equipment or technology, coordinate development and submittal of certification to TxDOT that toll systems technology complies with the interoperability rules that are in effect on the date of issuance of the request for proposals for the toll systems integrator contract (8.b.6 of PA)
- Coordinate development and submittal of letter to TxDOT acknowledging final acceptance of the toll collection system (8.b.7 of PA)

TASK 4. TOLL LANE MAINTENANCE AND OPERATIONS [GL Code 62854]

The GEC will provide technical support to the Mobility Authority as necessary to facilitate the completion of an agreement with TxDOT to provide the services and/or contractor(s) necessary to maintain and operate the SH 71 Toll Lanes roadway and emergency response items. This effort will include, but is not limited to, the following:

A. FINAL ACCEPTANCE

- Coordinate with TxDOT to identify maintenance limits and roles and responsibilities
- Coordination with TxDOT to identify a mutually agreed upon location in the corridor for maintenance operations and storage of materials needed for maintenance (7.b.4 from PA)

B. CONTRACT AD BUDGET DEVELOPMENT

- Support for the development of contracts to perform traffic management and safety operations upon final acceptance, including:
 - Police services in a manner consistent with the Mobility Authority's system
 - Wrecker services in a manner consistent with the Mobility Authority's system
 - Lane closures in a manner consistent with the Mobility Authority's system (9.b.12 of PA)
- Preparation of a detailed maintenance work element budget and annualized cost estimates.

TASK 5. COMMUNITY OUTREACH [GL Code 13750]

The GEC shall support the Mobility Authority as necessary with the Community Outreach program for the SH 71 Toll Lanes project. These services include, but are not limited to, the following:

A. AGENCY SUPPORT

- Coordinate with and support TxDOT with their Community Outreach program

B. PROJECT WEBSITE SUPPORT

- EITHER Support the Mobility Authority in the development and maintenance of the Project Website OR coordinate with TxDOT to maintain or update project website

C. MISCELLANEOUS SUPPORT

- Support the Mobility Authority in the development of miscellaneous communications activities
- Support the Mobility Authority in activities necessary for road operations

**Attachment B - Fee Estimate
Summary**

Mobility Authority General Engineering Consultant
Atkins - Man-hour Breakdown & Fee Estimate
SH 71 Toll Lanes Project

ATKINS - Work Authorization #11

SH 71 Toll Lanes Project

	<u>SUBTOTAL</u>	<u>SUBTOTAL</u>	<u>TOTAL</u>
TASK	Labor + Overhead + Profit	Direct Expenses	
SH 71 Toll Lanes Project			
1 Program Management and Administrative Support [GL Code 13720]	\$ 157,894	\$ 3,000	\$ 160,894
2 Design/Construction GEC Oversight [GL Code 13720]	\$ 785,657	\$ 14,500	\$ 800,157
3 Toll Systems Integrator Oversight [GL Code 13720]	\$ 63,883	\$ 1,200	\$ 65,083
4 Toll Lane Maintenance and Operations [GL Code 62854]	\$ 116,515	\$ 2,100	\$ 118,615
5 Community Outreach [GL Code 13750]	\$ 148,593	\$ 167,600	\$ 316,193
Subtotals	\$ 1,272,542	\$ 188,400	\$ 1,460,942
	TOTAL (rounded)		\$ 1,470,000

Attachment B - Fee Estimate

Mobility Authority General Engineering Consultant
 Atkins - Man-hour Breakdown & Fee Estimate
 SH 71 Toll Lanes Project

ATKINS - Work Authorization #11 SH 71 Toll Lanes Project

SH 71 Toll Lanes Project

TASK / WORK DESCRIPTION	(Estimated Average Labor Rates)						TOTAL HRS
	A	B	C	D	E	F	
1.0 Program Management and Administrative Support [GL Code 13720]							
A Project Administration					120	120	240
B Project Reporting	120	120					240
C Project Schedule				120			120
D Coordination Meetings	60	240					300

TOTAL DIRECT LABOR		180	360	0	120	120	120	900
<i>% Total by Classification</i>		20.00%	40.00%	0.00%	13.33%	13.33%	13.33%	
Labor Costs	\$ 14,400	\$ 25,200	\$ -	\$ 5,400	\$ 4,200	\$ 3,000	\$ 52,200	
Overhead Costs	1.7007 \$ 24,490	\$ 42,858	\$ -	\$ 9,184	\$ 7,143	\$ 5,102	\$ 88,777	
Profit	12.0% \$ 4,667	\$ 8,167	\$ -	\$ 1,750	\$ 1,361	\$ 972	\$ 16,917	
Total Loaded Labor	\$ 43,557	\$ 76,225	\$ -	\$ 16,334	\$ 12,704	\$ 9,074	\$157,894	

Direct Expenses	
Plotting and Reproduction	\$ 1,000
Mail and Deliveries	\$ 500
Misc Expenses	\$ 500
Travel and Field Expenses	\$ 1,000
Total Direct Expenses	\$ 3,000

Total \$ 160,894

Attachment B - Fee Estimate

Mobility Authority General Engineering Consultant
 Atkins - Man-hour Breakdown & Fee Estimate
 SH 71 Toll Lanes Project

ATKINS - Work Authorization #11 SH 71 Toll Lanes Project

SH 71 Toll Lanes Project

TASK / WORK DESCRIPTION	(Estimated Average Labor Rates)						TOTAL HRS
	A	B	C	D	E	F	
	\$ 80.00	\$ 70.00	\$ 60.00	\$ 45.00	\$ 35.00	\$ 25.00	
2.0 Design/Construction GEC Oversight [GL Code 13720]							
A Management and General Technical Support	240	480		480	480		1680
B Document Reviews	240	480		480	480		1680
C Construction Oversight Inspection of Tolled Lanes				960		120	1080
D Requests For Information (RFI) and Non-Conformance Report Processing				120		60	180
E Shop Drawing/Submittals Processing and Management		120		120			240
F Mobility Authority Construction Coordination Support	60	60					120
G Notice of Completion				32			32

TOTAL DIRECT LABOR	540	1140	0	2192	960	180	5012
<i>% Total by Classification</i>	<i>10.77%</i>	<i>22.75%</i>	<i>0.00%</i>	<i>43.74%</i>	<i>19.15%</i>	<i>3.59%</i>	
Labor Costs	\$ 43,200	\$ 79,800	\$ -	\$ 98,640	\$ 33,600	\$ 4,500	\$ 259,740
Overhead Costs	1.7007 \$ 73,470	\$ 135,716	\$ -	\$ 167,757	\$ 57,144	\$ 7,653	\$ 441,740
Profit	12.0% \$ 14,000	\$ 25,862	\$ -	\$ 31,968	\$ 10,889	\$ 1,458	\$ 84,178
Total Loaded Labor	\$ 130,671	\$ 241,378	\$ -	\$ 298,365	\$ 101,633	\$ 13,612	\$785,657

Direct Expenses	
Plotting and Reproduction	\$ 2,000
Mail and Deliveries	\$ 500
Misc Expenses	\$ 3,000
Travel and Field Expenses	\$ 9,000
Total Direct Expenses	\$ 14,500

Total \$ 800,157

Attachment B - Fee Estimate

Mobility Authority General Engineering Consultant
 Atkins - Man-hour Breakdown & Fee Estimate
 SH 71 Toll Lanes Project

ATKINS - Work Authorization #11 SH 71 Toll Lanes Project

SH 71 Toll Lanes Project

		<i>A</i>	<i>B</i>	<i>C</i>	<i>D</i>	<i>E</i>	<i>F</i>	TOTAL
	(Estimated Average Labor Rates)	\$ 80.00	\$ 70.00	\$ 60.00	\$ 45.00	\$ 35.00	\$ 25.00	HRS
TASK / WORK DESCRIPTION								
3.0 Toll Systems Integrator Oversight [GL Code 13720]								
A Design and Construction Oversight				288				288
B Final Acceptance				64				64

TOTAL DIRECT LABOR		0	0	352	0	0	0	352
<i>% Total by Classification</i>		<i>0.00%</i>	<i>0.00%</i>	<i>100.00%</i>	<i>0.00%</i>	<i>0.00%</i>	<i>0.00%</i>	<i>0.00%</i>
Labor Costs	\$	-	\$ -	\$ 21,120	\$ -	\$ -	\$ -	\$ 21,120
Overhead Costs	1.7007 \$	-	\$ -	\$ 35,919	\$ -	\$ -	\$ -	\$ 35,919
Profit	12.0% \$	-	\$ -	\$ 6,845	\$ -	\$ -	\$ -	\$ 6,845
Total Loaded Labor	\$	-	\$ -	\$ 63,883	\$ -	\$ -	\$ -	\$63,883

Direct Expenses		
Plotting and Reproduction	\$	100
Mail and Deliveries	\$	100
Misc Expenses		
Travel and Field Expenses	\$	1,000
Total Direct Expenses	\$	1,200

Total \$65,083

Attachment B - Fee Estimate

Mobility Authority General Engineering Consultant
 Atkins - Man-hour Breakdown & Fee Estimate
 SH 71 Toll Lanes Project

ATKINS - Work Authorization #11 SH 71 Toll Lanes Project

SH 71 Toll Lanes Project

		A	B	C	D	E	F	TOTAL
		(Estimated Average Labor Rates)						
		\$ 80.00	\$ 70.00	\$ 60.00	\$ 45.00	\$ 35.00	\$ 25.00	HRS
TASK / WORK DESCRIPTION								
4.0	Toll Lane Maintenance and Operations [GL Code 62854]							
A	Develop Maintenance Plan	24	48		120			192
B	Contract and Budget Development	48	96		384			528

TOTAL DIRECT LABOR		72	144	0	504	0	0	720
<i>% Total by Classification</i>		<i>10.00%</i>	<i>20.00%</i>	<i>0.00%</i>	<i>70.00%</i>	<i>0.00%</i>	<i>0.00%</i>	
Labor Costs	\$	5,760	\$ 10,080	\$ -	\$ 22,680	\$ -	\$ -	\$ 38,520
Overhead Costs	1.7007 \$	9,796	\$ 17,143	\$ -	\$ 38,572	\$ -	\$ -	\$ 65,511
Profit	12.0% \$	1,867	\$ 3,267	\$ -	\$ 7,350	\$ -	\$ -	\$ 12,484
Total Loaded Labor	\$	17,423	\$ 30,490	\$ -	\$ 68,602	\$ -	\$ -	\$116,515

Direct Expenses	
Plotting and Reproduction	\$ 500
Mail and Deliveries	\$ 100
Misc Expenses	\$ 500
Travel and Field Expenses	\$ 1,000
Total Direct Expenses	\$ 2,100

Total \$ 118,615

Attachment B - Fee Estimate

Mobility Authority General Engineering Consultant
 Atkins - Man-hour Breakdown & Fee Estimate
 SH 71 Toll Lanes Project

ATKINS - Work Authorization #11 SH 71 Toll Lanes Project

SH 71 Toll Lanes Project

		A	B	C	D	E	F	TOTAL
	(Estimated Average Labor Rates)	\$ 80.00	\$ 70.00	\$ 60.00	\$ 45.00	\$ 35.00	\$ 25.00	HRS
TASK / WORK DESCRIPTION								
5.0 Community Outreach [GL Code 13750]								
A Agency Support					100	200	200	500
B Project Website Support					75	150	300	525
C Miscellaneous Support					100	200	200	500

		0	0	0	275	550	700	1525
	<i>% Total by Classification</i>	0.00%	0.00%	0.00%	18.03%	36.07%	45.90%	
TOTAL DIRECT LABOR								
Labor Costs		\$ -	\$ -	\$ -	\$ 12,375	\$ 19,250	\$ 17,500	\$ 49,125
Overhead Costs	1.7007	\$ -	\$ -	\$ -	\$ 21,046	\$ 32,738	\$ 29,762	\$ 83,547
Profit	12.0%	\$ -	\$ -	\$ -	\$ 4,011	\$ 6,239	\$ 5,671	\$ 15,921
Total Loaded Labor		\$ -	\$ -	\$ -	\$ 37,432	\$ 58,227	\$ 52,934	\$148,593

Direct Expenses	
Plotting and Reproduction	\$ 500
Mail and Deliveries	\$ 100
Misc Expenses	\$ 1,000
Travel and Field Expenses	\$ 1,000
Advertising	\$ 75,000
Direct Mailers	\$ 25,000
Toll Bill Insert/PBM Support	\$ 15,000
Grand Opening	\$ 50,000
Total Direct Expenses	\$ 167,600

Total \$ 316,193



**CENTRAL TEXAS
Regional Mobility Authority**

AGENDA ITEM #11 SUMMARY

Approve a work authorization with Telvent USA, LLC, for toll system design and integration services for the SH 71 Express Project.

Strategic Plan Relevance: Regional Mobility

Department: Toll Operations

Associated Costs: \$2,059,045

Funding Source: General Fund, Reimbursed per Project Agreement with TxDOT

Board Action Required: Yes

Description of Matter: Schneider Electric (Telvent USA LLC) will provide Tolls System Integration services related to project activities required to assist the Mobility Authority in the development of the SH 71 Toll Lanes. These efforts will include, but not be limited to, procurement, installation, testing, and implementation of a complete and fully operational toll collection system. Services will also include communications and system interfaces consisting of design, coordination, and project interface activities to facilitate the design and construction of the toll system infrastructure facilities by others on the SH 71 Toll Lanes Project, and additional activities as specifically requested by the Authority.

Reference documentation: Draft Resolution
Proposed Work Authorization

Contact for further information: Tim Reilly, Director of Operations

**GENERAL MEETING OF THE BOARD OF DIRECTORS
OF THE
CENTRAL TEXAS REGIONAL MOBILITY AUTHORITY**

RESOLUTION NO. 14-___

**APPROVING A WORK AUTHORIZATION WITH TELVENT USA, LLC,
FOR TOLL SYSTEM DESIGN AND INTEGRATION SERVICES
FOR THE SH 71 EXPRESS PROJECT.**

WHEREAS, the Central Texas Regional Mobility Authority (“Mobility Authority”) entered into a contract with Caseta Technologies, Inc. dated April 27, 2005, for the design, procurement, and installation of a toll collection system on the Authority’s turnpike system (the “Contract”); and

WHEREAS, Caseta Technologies, Inc., was subsequently acquired by Telvent USA Corporation, a Maryland corporation (“Telvent”), and all rights and obligations of Caseta Technologies, Inc. under the Contract are now the rights and obligations of Telvent; and

WHEREAS, Telvent is providing toll system implementation services for the MoPac Improvement Project and other projects under work authorizations previously authorized by the Board under the Contract; and

WHEREAS, the Executive Director and Telvent have discussed and agreed to a proposed work authorization for Telvent to provide toll system design and integration services for the SH 71 Express Project; and

WHEREAS, the Executive Director recommends approval of the proposed work authorization attached as Exhibit 1.

NOW THEREFORE, BE IT RESOLVED that the proposed work authorization is hereby approved; and

BE IT FURTHER RESOLVED that the Executive Director may finalize and execute for the Mobility Authority the proposed work authorization in the form or substantially the same form as Exhibit 1.

Adopted by the Board of Directors of the Central Texas Regional Mobility Authority on the 30th day of July, 2014.

Submitted and reviewed by:

Approved:

Andrew Martin
General Counsel for the Central
Texas Regional Mobility Authority

Ray A. Wilkerson
Chairman, Board of Directors
Resolution Number: 14-___
Date Passed: 7/30/2014

EXHIBIT 1 TO RESOLUTION 14-
PROPOSED WORK AUTHORIZATION

[on the following 49 pages]

CENTRAL TEXAS REGIONAL MOBILITY AUTHORITY

WORK AUTHORIZATION

WORK AUTHORIZATION NO. 12

TOLL SYSTEM IMPLEMENTATION

SH 71 TOLL LANES PROJECT

THIS WORK AUTHORIZATION (“WA No. 12”) is made pursuant to the terms and conditions of Article 1 of the GENERAL PROVISIONS, Attachment A to the original Contract for Toll System Implementation, dated April 27, 2005 (the Contract) entered into by and between the Central Texas Regional Mobility Authority (the “Authority” or “CTRMA”), and TELVENT USA, LLC (the “Contractor,” also referred to in attachments to this WA No. 12 as the “System Integrator” or “SI”).

PART I. The Contractor will perform toll implementation services generally described in the Scope of Work attached hereto as **Attachment A**. The Contractor’s duties and responsibilities are further detailed in: (1) the SH 71 Toll Lanes Project Layout included as **Attachment B**, (2) the Toll Facility Responsibility Matrix included as **Attachment C**, and (3) the Fixed Price Tolling Standards included as **Attachment D**.

PART II. The maximum amount payable under this WA No. 12 is \$ 2,059,495. This amount is based upon the pricing obtained, and is documented by the fee schedule set forth in **Attachment E**

PART III. Payment to the Contractor for the services established under this WA No. 12 shall be made in accordance with the Contract.

PART IV. This WA No. 12 shall become effective on the date both parties have signed this WA No. 12. This WA No. 12 will terminate on the SH 71 Toll Lanes substantial completion date or upon payment of the maximum amount payable in **Part II**, whichever date is first, unless extended as provided by the Contract. The work shall be performed in accordance with the Project Schedule and Milestones as set forth in **Attachment F**.

PART V. This WA No. 12 does not waive any of the parties' responsibilities and obligations provided under the Contract, and except as specifically modified by this WA No. 12, all such responsibilities and obligations under the Contract remain in full force and effect.

IN WITNESS WHEREOF, this Work Authorization No. 12 is executed in duplicate counterparts and hereby accepted and acknowledged below.

THE CONTRACTOR: Telvent USA, LLC.

Signature Date

Typed/Printed Name and Title

CENTRAL TEXAS REGIONAL MOBILITY AUTHORITY

Executed for and approved by the Central Texas Regional Mobility Authority for the purpose and effect of activating and/or carrying out the orders, established policies or work programs heretofore approved and authorized by the Texas Transportation Commission.

Signature Date

Mike Heiligenstein, Executive Director

Typed/Printed Name and Title

LIST OF ATTACHMENTS

- Attachment A Scope of Work
- Attachment B SH 71 Toll System Layout
- Attachment C Toll Facility Responsibility Matrix
- Attachment D Fixed Price Tolling Standards
- Attachment E Fee Schedule/Budget
- Attachment F Preliminary Project Schedule and Milestones

ATTACHMENT A***CENTRAL TEXAS REGIONAL MOBILITY AUTHORITY***
TOLL SYSTEM IMPLEMENTATION
State Highway 71 Toll Lanes Project**SCOPE OF WORK for SYSTEMS INTEGRATOR****A1.0 General****A1.01. Background**

The Texas Department of Transportation (“TxDOT”) is developing the SH 71 Toll Lanes Project (“Project”), which will consist of adding toll lanes to SH 71 from Presidential Boulevard to east of SH 130, and will include the realignment of FM 973 where that road intersects with SH 71. The project length is approximately 4 miles. TxDOT will be developing the project in cooperation with the Central Texas Regional Mobility Authority (“Authority”), with TxDOT taking the lead in the design, permitting, and construction of the Project, and the Authority responsible for the design, procurement, permitting, installation, testing and commissioning of the Toll Collection System (TCS), which will include but not necessarily be limited to tolling equipment, cameras, antennas, fiber optic system, and the supporting electrical system.

Upon substantial completion, the Authority shall operate and maintain toll lanes on the Project, which will include the collection of tolls, setting toll rates, servicing customers, toll enforcement, facilities and toll collection system maintenance, repairs and capital improvements to the toll lanes, toll facilities, and related equipment. TxDOT shall operate and maintain the general purpose lanes and the FM 973 realigned intersection with SH 71.

A1.02. Summary Scope of Work

The Scope of Work for Work Authorization No. 12 provides for the procurement, installation, testing, and implementation of a complete and fully operational TCS for the Project by the Systems Integrator (SI), including all of the required communications and systems interfaces including design, coordination, and project interface activities to facilitate the design and construction of the toll system infrastructure facilities by others on the SH 71 Toll Lanes Project.

This Work Authorization also authorizes the SI to establish and maintain relationships with a wide variety of third parties and to coordinate the designs for the proposed TCS with the entire Hwy 71 Toll Project to ensure that the construction of the toll system infrastructure facilities will be fully compatible and will meet the requirements for the CTRMA’s TCS. In this role, the SI will work closely with CTRMA, TxDOT, and various designers and roadway contractors in developing the required complete TCS and network infrastructure.

A2.0 General Description – Toll Road Infrastructure and Site

The SH 71 Toll Lanes Project limits extend from Presidential Boulevard to east of SH 130, and will include the realignment of FM 973 where that road intersects with SH 71. The project length is approximately 4 miles.

The existing roadway in the vicinity of the preliminary location of the toll gantries includes three 12-ft lanes in each direction with a depressed grassed median. Right-of-way width varies from 220 to 280 feet.

Proposed Facility: The proposed work for the entire toll road facility will consist of the following:

- Adding two new toll lanes (one in each direction) from Presidential Boulevard to SH 130; Number of toll lanes approaching FM 973 and at the gantry location increases to four lanes (two in each direction), and
- Constructing bridges over FM 973 and SH 130 and connecting ramps between the new express toll lanes and the mainlanes of SH 71 and SH 130; and
- Widening of SH 71 between Presidential Boulevard and FM 973

The Toll Collection System (TCS) for the Project will be all Electronic Toll Collection (ETC). The entire full build project will consist of four (4) gantry lanes at the locations listed in Table 1 below. Locations are approximate and may be subject to change as the CDA Developer progresses towards the completion of plans development.

Table 1: Gantry Locations and Lane Counts

Approximate Station Location	Direction of Travel	No. of Lanes	No. of Shoulders (8' or greater)	Comments
11150+00	Westbound	2	2	The preliminary schematic typical section includes 10 foot shoulders on either side. However, the typical section may be different if the location of the gantry is revised.
11150+00	Eastbound	2	2	The preliminary schematic typical section includes 10 foot shoulders on either side. However, the typical section may be different if the location of the gantry is revised.
Total Gantry Lanes		2	4	

Refer to the SH 71 Toll Lanes Project Layout included as **ATTACHMENT B** for the general project layout.

A3.0 General Requirements - Toll Collection System

The TCS for the CTRMA Turnpike System, which is being designed and implemented through a series of separate work authorizations for the various segments of the proposed Toll Road System, generally will be fully compatible with the TCS which has been designed and implemented for the 183A Toll Road and the Manor Expressway Projects, using automatic vehicle identification and classification technology, a Violation Enforcement System (VES) with an integrated camera and triggering system to capture referenced digital images of license plates, and a Remote Online Management System (ROMS). It is required that the TCS be interoperable with the other Texas ETC systems.

The Customer Service Center (CSC) is located in a facility at 12719 Burnet Road, Austin, Texas, developed and administrated by the Toll Operations Division (TOD) of TxDOT. The CTRMA contracts with the members of the Texas Statewide Interoperability Task force for CSC services for its customers. Expansion of CTRMA's TCS to serve the Hwy 71 Toll Project includes coordination and design of appropriate interfaces with the CSC. Appropriate communications links between the various toll facilities on the CTRMA Toll Road System and the CTRMA Administrative Offices, the future Traffic Management Center (TMC) at the Field Operations Building(s) and the Violation Processing Center (VPC) are part of the requirements of the design/implementation work.

The VPC is located in a separate facility, and is being administrated by the Municipal Services Bureau, Inc. under contract to the CTRMA. Development of CTRMA's TCS also will include coordination and design of appropriate interfaces with the VPC. Appropriate communications links between the various toll facilities on the CTRMA Toll Road System, the CTRMA Administrative Offices, the 183A Field Operations Building and the CSC are part of the requirements of the design/implementation work.

A4.0 Equipment and Installation – Gantries and Roadside Equipment

For all TCS field installations on the various segments of the Hwy 71 Toll Project, the SI will be required to provide and install the toll equipment systems and hardware for a complete, tested, and operating TCS under this Work Authorization. The principle items of work and primary components of the TCS at each Remote Toll Location will include, but are not limited to:

- Furnish & Install Lane Controllers and ancillary devices
- Furnish & Install ETC Lane components, including AVDS, AVC, VES, TSI and AVI systems and hardware.
- Furnish & Install all ETC Lane Equipment wiring & cable, hardware, brackets, and fasteners required to attach the ETC equipment to the gantries provided by the others.
- Furnish & Install ROMs monitoring for all ETC site equipment (i.e.: ETC Equipment, AVDS, AVC, AVI, VES, HVAC, generators, power, communications equipment, etc)
- Communication System Outside Fiber Optic Cable Plant, Inside Cable Plant, and Network Components (i.e.: Fiber Optic Cable, Terminations, Switches, routers and other network devices)
- Furnish & Install Master Ground System connected to the Master Ground Bus Bar provided by others
- Furnish & Install Lightning Surge Suppression System & Components for AVI, network, VES, UPS power, and service/feeder power.
- Furnish & Install Backup Electrical Power including Emergency Generators, Fuel Tanks, and Automatic Transfer Switches.
- Furnish & Install Uninterruptible Power Supply, including wiring & cable, hardware, and ROMs interface
- Furnish & Install In-Lane Processor (ILP) enclosure, with HVAC for appropriate environmental protection and climate controls for electronic equipment. Furnish & Install Site Surveillance Cameras & Security Systems to monitor each ILP and gantries.
- Provide power from the electrical service to the toll locations
- Federal Communication Commission (FCC) License preparation and submission
- Provide complete testing, certification and acceptance of all systems for complete, fully operational TCS, furnished and installed.

The procurement, fabrication and installation of gantries for the TCS to be located on the segments of the Project will be by others. It is the responsibility of the SI, nevertheless, to work closely with CTRMA, TxDOT, and the various designers and roadway contractors to establish the precise locations for each of the gantry structures and to provide the Roadway Contractor(s) with detailed information of the installation for the TCS equipment at each location.

A5.0 Coordination and Project Interface

The SI is to participate in the process for coordination which will enable the contractors and designers of the SH 71 Toll Lanes Project to obtain specific, detailed information regarding the proposed TCS components in order to complete the design/construction of the appropriate toll facilities infrastructure. The SI will be responsible for maintaining relationships with a wide variety of third parties, including designers, roadway contractors, and various suppliers. In this role, the SI will work closely with CTRMA and TxDOT in developing the required network. The work related to this Work Authorization No. 12 generally will include, but not be limited to:

- Design input and providing detailed information including TCS component details, dimensions and layout configurations, and specific technical requirements for elements of the proposed TCS;
- Preparation of construction/installation guidelines for various components of CTRMA's TCS;
- Review of construction documents prepared by others;
- Attendance and participation at coordination meetings as determined by project schedule and/or as requested by the CTRMA. This includes attending design coordination meetings, construction meetings, and issue resolution meetings as necessary to resolve outstanding comments
- Provide "over the shoulder" reviews, as necessary
- Submit Installation Plan and Installation Drawings to the CTRMA for review and approval
- Provide input in the development of the project schedule as it relates to the installation and testing of the toll system. The SI shall review the project baseline schedule prepared by the D/B contractor for review and acceptance.

All TCS infrastructure facilities at the remote Toll Locations will be provided by others as indicated in **Section A6.0 and Section A7.0** hereof. The SI shall fully coordinate the designs for the TCS with others and provide the required details and technical requirements to ensure that the construction of the toll system infrastructure facilities will be fully compatible and meet the requirements for the CTRMA's TCS.

The SI is responsible for coordinating with others and for providing all necessary details, system requirements, and reviews of construction documents to ensure that the gantries are located and configured properly to accommodate the SI's own particular system components as required to meet the CTRMA TCS performance and accuracy requirements.

Prior to deploying any toll collection equipment or technology the SI shall certify to TxDOT that the technology complies with the interoperability rules that are in effect on the date of issuance of the request for proposals for the toll systems integration contract.

A6.0. Work by Others – Civil/Roadway Construction

The CTRMA, through its roadway construction contracts, will provide jointed concrete pavement in each of the areas designated for toll collection facilities. The pavement will be reinforced with Glass Fiber

Reinforced Polymer (GFRP) bars. Transverse joints and longitudinal joints will be placed at positions equal to lane widths and as shown on the CTRMA details. Power and communication lines to support the Wide Area Network (WAN) will be provided by others and terminated at an ILP enclosure in an area within 500 feet of ILP. The SI is responsible for the communication links between the Host, the CSC, the VPC, the future TMC, and all Remote Express Toll Location facilities via a Communication Trunkline and WAN.

Except as may be expressly indicated elsewhere, all toll system infrastructure required for the TCS at the designated remote Express Toll Locations will be provided and installed by others. The principle items of work and primary components of the TCS infrastructure at each remote Express Toll Location shall include, but are not limited to:

- GFRP Bar Reinforced Pavement Section;
- Retaining Walls and Coping Details;
- Drainage Features;
- Civil Site Work, including Grading, Access Driveways, and Fencing;
- All toll gantry procurement and installations, including foundations and gantry structures;
- ILP concrete foundation slab. The ILP's are to be provided with appropriate environmental protection and climate controls for housing the electronic equipment by the SI;
- Conduit and ground boxes providing connections between the ILP's and the ETC Lane equipment installations. NOTE: It is the responsibility of the SI to coordinate with the Roadway Contractor(s) for the placement and installation of these elements to ensure that the construction is acceptable for the TCS as designed;
- Gantry and ILP enclosure lightning protection air, terminal, Down Conductors, ILP Master Bus Bar, and Ground Electrodes. Equipment connection to the Ground Electrode for the ILP enclosure Master Ground Bus Bar will be provided by Others;
- Power and WAN communication services up to the location of the proposed ILP enclosures;
- Provide, install, and incorporate natural gas lines, if available. NOTE: SI is to coordinate and provide generator requirements including locations for gas feeds for the Emergency Generators;
- Concrete foundations for Emergency Generators and associated fuel tanks; and
- All signing, pavement markings, traffic barriers and other roadway appurtenances required at each remote Express Toll Location.

Refer to the Fixed Price Tolling Standards that were issued by the CTRMA on November 2013, which is included as *ATTACHMENT D*.

A7.0 Toll Facilities Responsibility Matrix

For this work authorization, the SI is responsible for design and coordination of the various aspects of the TCS as identified in *ATTACHMENT C - Toll Facilities and ITS Responsibility Matrix*, and shall work with the CTRMA, TxDOT, roadway designers and contractors, and others as described herein.

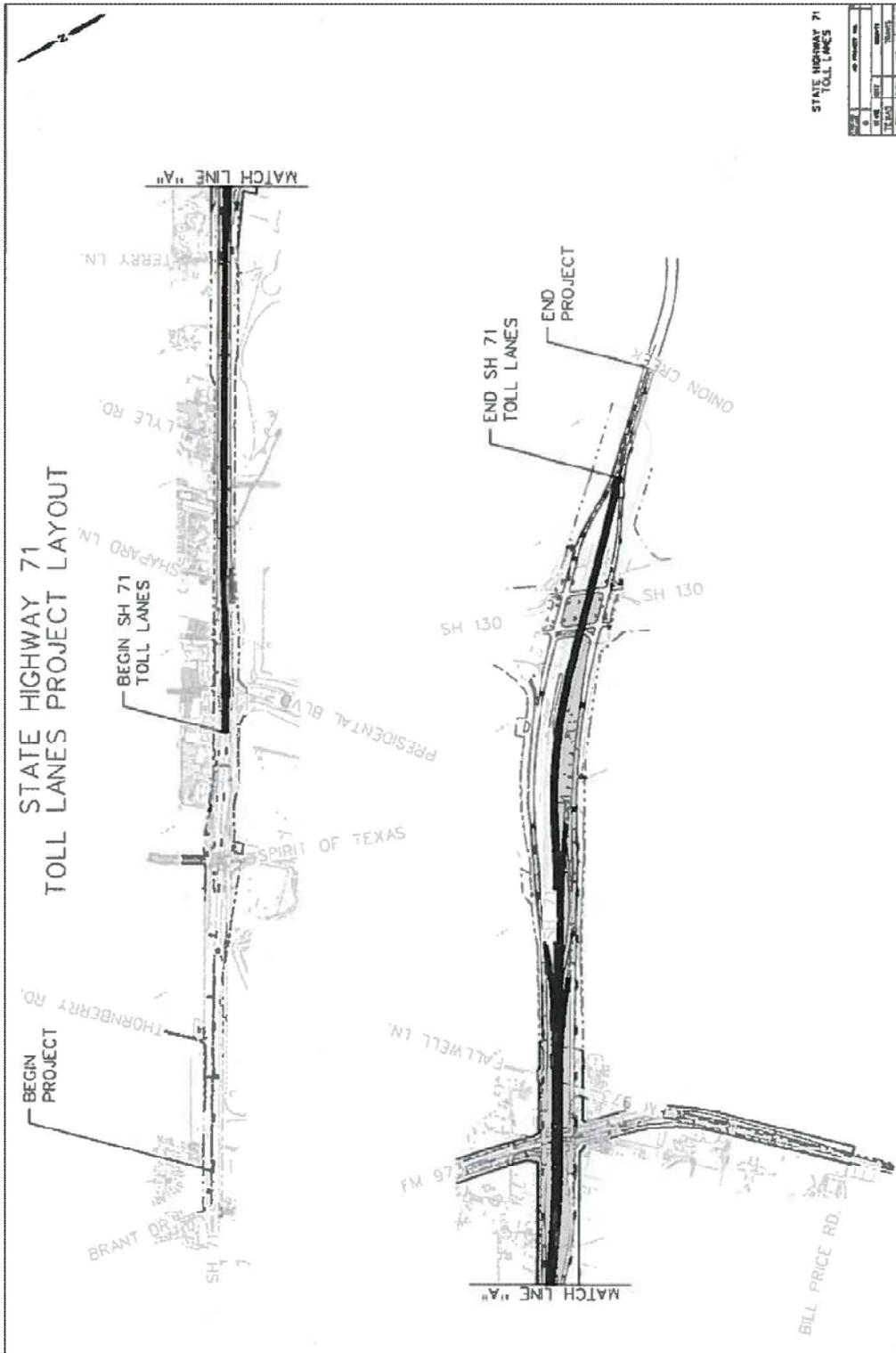
A8.0 Project Schedule

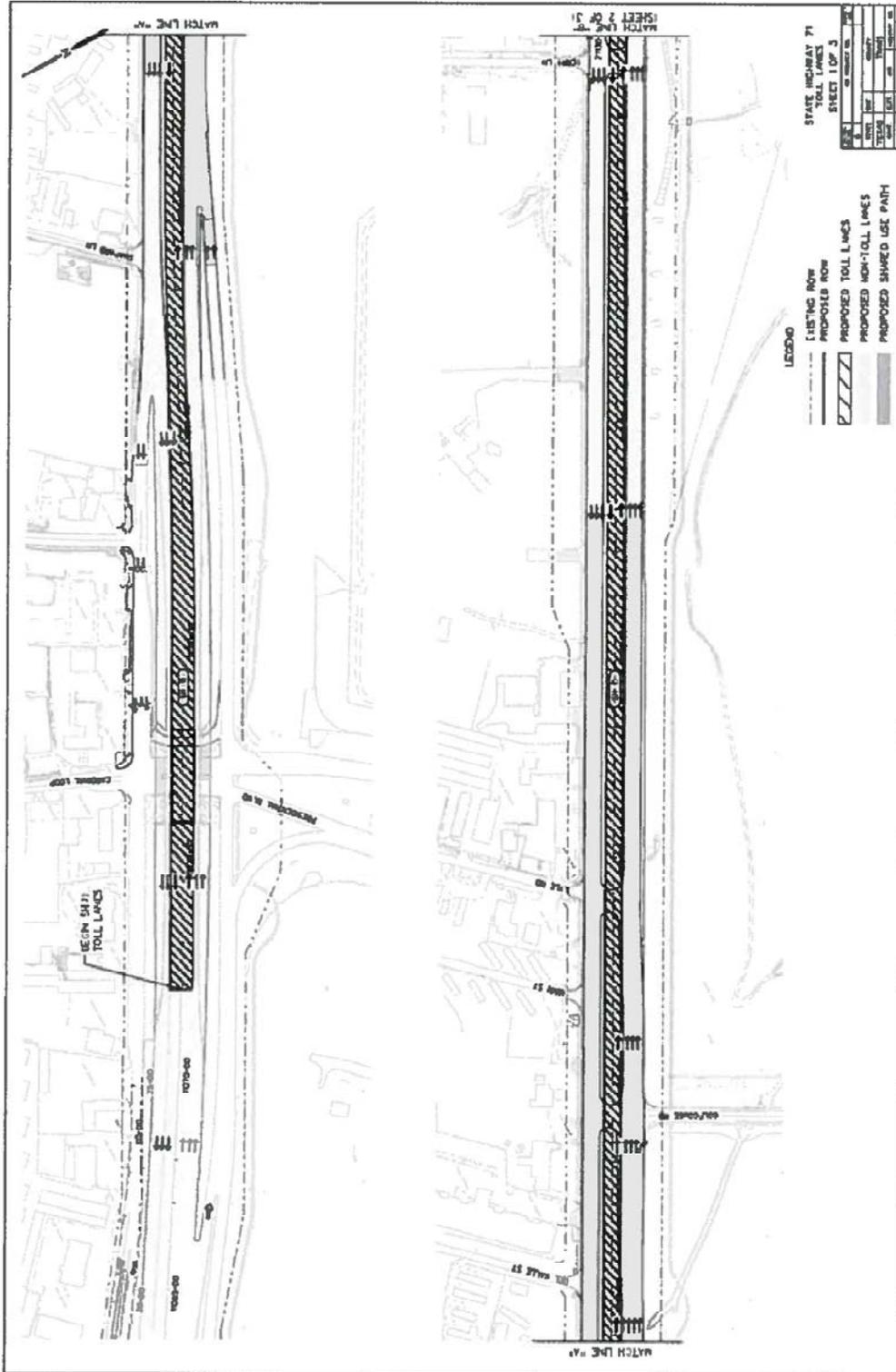
The Project Schedule shall be developed to incorporate the Milestone Dates established for this Work Authorization No. 12 as presented in *ATTACHMENT F*.
[END OF SECTION]

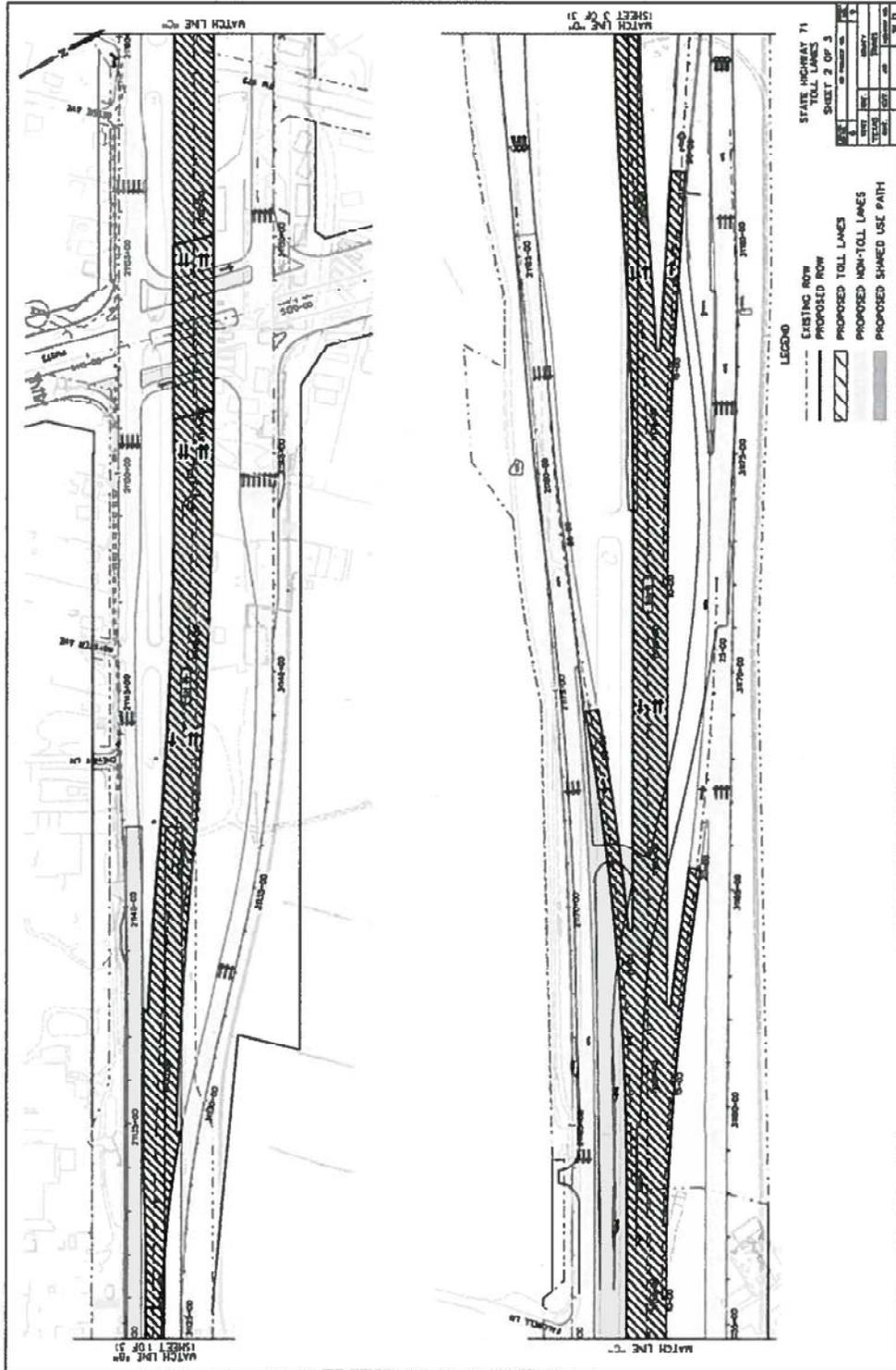
ATTACHMENT B

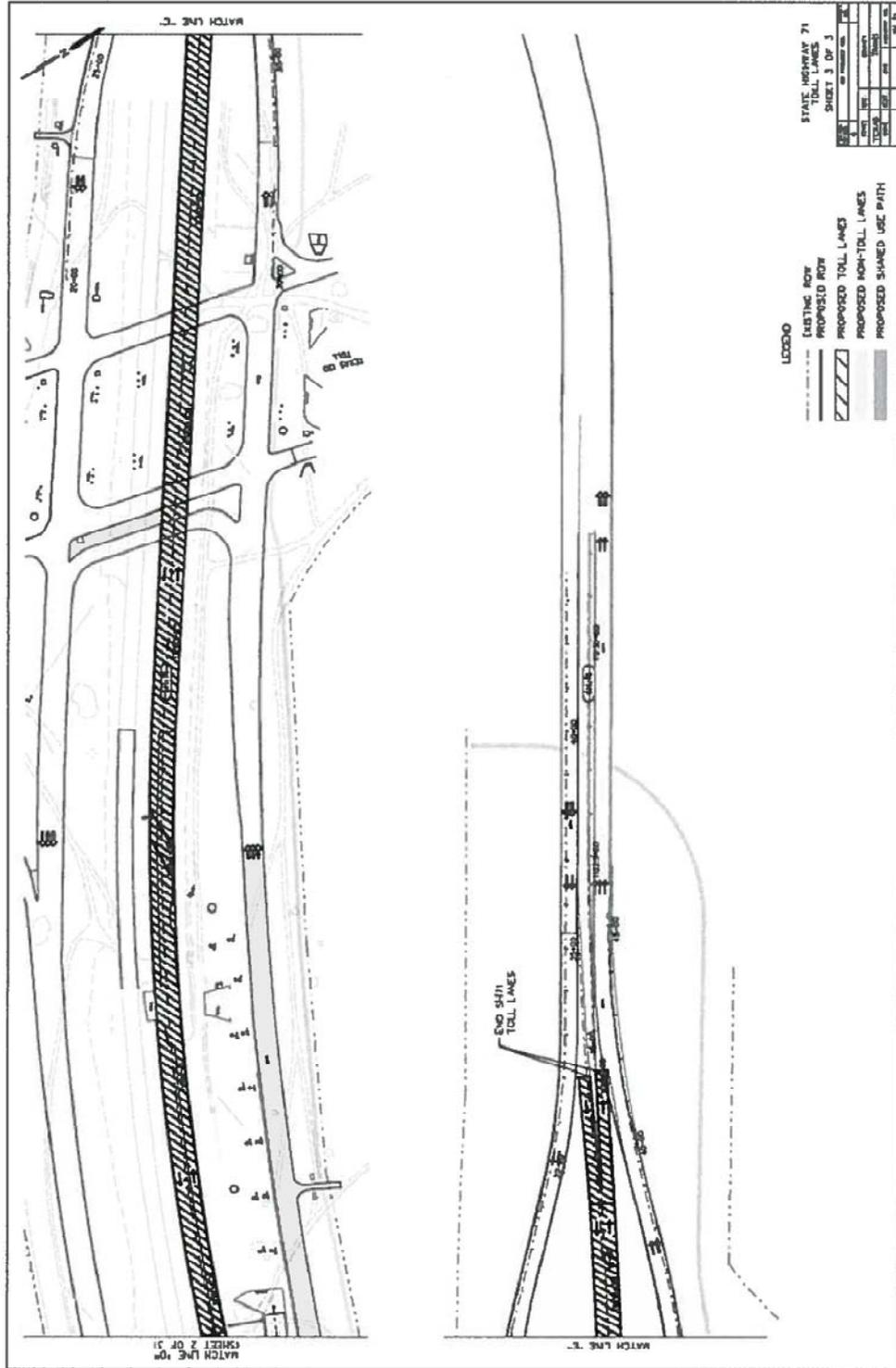
TOLL SYSTEM LAYOUT

State Highway 71 Toll Lanes









TOLL FACILITY RESPONSIBILITY MATRIX

Responsibility Assignment Legend							
Primary Responsibility: P	Support Responsibility: S		Coordination Responsibility Only: C			No Responsibility: N	
Element/Task/Component/ Sub-system	DB Contractor (DB)			Systems Integrator (SI)			Comments Other Responsibility/Information
	Design	Procure	Install/ Construct	Design	Procure	Install/ Construct	
GENERAL REQUIREMENTS							
Schedule	P	P	P	S	C	S	DB must accommodate and incorporate the SI scheduled activities into the DB schedule. All schedule changes or updates which impact the SI tasks must be agreed to by the SI prior to submittal to the Mobility Authority. A weekly schedule must be distributed and incorporate any SI updates or changes.
Request for Early Opening	P	P	P	S	S	S	SI must be able to match schedule request for early opening to conform to requirements in RFDP.
Design Package – Installation and Electrical Design and Plans	P	P	P	C	N	C	DB to incorporate all SI requirements and specifications into Structural and Electrical Design Packages. SI to provide approval prior to issuance of Released For Construction (RFC) plans.
Grading	P	P	P	C	N	C	
Drainage	P	P	P	C	N	C	No culverts or pipes under tolling zones.
Utilities/Electrical Services	P	P	P	S	C	C	SI to provide specific power requirements for the Toll System. DB to incorporate into toll facilities design and construct power utilities interface, and all power infrastructure.
Traffic Control/Safe work zone	P	P	P	S	N	C	SI to provide DB detailed lane closure requirements and schedule for installation and testing.
Signing	P	P	P	C	N	N	All toll signing must be coordinated with and approved by the Mobility Authority. If toll price signs utilize changeable electronic signs, the DB will provide the static sign and the SI will provide the electronic insert.
Striping	P	P	P	S	N	C	SI to coordinate striping with pavement loop locations.
Lighting	P	P	P	S	C	S	Roadway and toll location lighting provided by DB. SI to provide lighting requirements in vicinity of toll locations and locations of other Toll System equipment. DB to confirm that lighting does not obstruct toll related signing or impede the Toll System.

TOLL FACILITY RESPONSIBILITY MATRIX
SH 71 Project

DRAFT

Responsibility Assignment Legend							
Primary Responsibility: P	Support Responsibility: S			Coordination Responsibility Only: C			No Responsibility: N
Element/Task/Component/ Sub-system	DB Contractor (DB)			Systems Integrator (SI)			Comments Other Responsibility/Information
	Design	Procure	Install/ Construct	Design	Procure	Install / Construct	
Landscaping	P	P	P	C	N	N	
Fencing/Guardrail/Bollards/Concrete Barrier	P	P	P	S	C	C	SI to provide requirements for specific equipment clearances for Toll System. DB to incorporate into roadway design. SI to confirm that design plans meet requirements.
TOLL SYSTEM: LOCATIONS, LAYOUTS, STRUCTURES, MOUNTS/BRACKETS							
Locations and Layouts	P	P	P	S	C	C	SI to provide specific locations for the Toll System, SI to provide requirements for specific lane and facility layouts. DB to incorporate into Design Packages. SI to review and approve.
Gantries/Foundation/Trusses/Junction boxes/Conduits/Grounding	P	P	P	S	C	S	SI to provide requirements for conduits (for SI installed power and communications cables, including specific requirement for below ground conduits for the loops), junction boxes, and power needs for the Toll System. DB to incorporate into structural design, including electrical grounding, bonding. DB to provide and install junction boxes and conduit pull strings and bell ends for all conduits up to one foot above pole and gantry foundation. The DB will require SI to sign off on below-ground conduits for the loops prior to installation of special pavement structure.
Gantries/Foundation/Trusses/Junction boxes/Conduits/Grounding	S	C	S	P	P	P	SI to install conduits from one foot above grade to all Toll System components.
Equipment Mounts on Brackets/Frames	S	N	C	P	P	P	SI to procure and install all Toll System equipment, and related cable & wiring, including communications from roadside cabinets to the equipment mounted on the gantries. SI to provide requirements for all brackets and frames needed to attach SI procured equipment to DB provided truss.
Equipment Brackets/Frames on Gantries	P	P	P	S	N	C	DB to provide and install all frames needed to attach all SI procured equipment. SI to provide locations for installation to the DB. SI to provide and install all mounting brackets required for tolling equipment.

TOLL FACILITY RESPONSIBILITY MATRIX
SH 71 Project

DRAFT

Responsibility Assignment Legend							
Primary Responsibility: P	Support Responsibility: S		Coordination Responsibility Only: C			No Responsibility: N	
Element/Task/Component/ Sub-system	DB Contractor (DB)			Systems Integrator (SI)			Comments Other Responsibility/Information
	Design	Procure	Install/ Construct	Design	Procure	Install / Construct	
Pavement structure, including special nonferrous zones and conduit stub-outs for in-pavement sensors/loops	P	P	P	S	N	C	SI to provide requirements for special pavement structure at toll gantry areas. SI shall coordinate joint spacing to avoid conflicts with loop placement and sign off on riser locations before concrete pour. DB to assure ferrous objects (i.e. rebar, grates, pipes, etc.) are not in toll revenue collection detection system(s) zone of influence. DB to locate loop risers after pavement is poured.
EQUIPMENT CABINETS							
Toll Equipment Cabinets	C	N	S	P	P	P	SI to provide size and number of cabinets needed for Toll System. DB shall incorporate location into site grading and drainage. SI to procure and install environmentally controlled cabinets. The environmentally controlled enclosures provided by SI must comply with the America Society of Heating, Refrigeration, and Air Conditioning Engineers: Thermal Guidelines for Data Processing Environments. DB to provide traffic control devices and safe working conditions for SI during installation of all toll equipment.
Toll Equipment Cabinet Site (TEC) and Roadside Equipment Cabinet Base Slabs	P	P	P	S	N	C	SI to provide requirements for specific equipment weight and anchorages for cabinets to the DB. DB to incorporate into Roadway Design. DB to install slabs with conduit plumbing.
Facility Security and Security Communications at Toll System locations	C	N	C	P	P	P	SI to provide security communications for all toll system equipment. DB to incorporate into the Roadway Design.
TOLL SUB-SYSTEMS							
Automatic Vehicle Identification (AVI) Antennas and Readers	N	N	S	P	P	P	SI to provide AVI System Mounts, Wiring and Cables. SI will perform all AVI system installation and terminations, and to make the connections to the electronics in the cabinets.
Automatic Vehicle Classification and Detection (AVC) and (AVD)	N	N	S	P	P	P	SI to install, connect and terminate AVC and/or AVD System mounted on the gantries and/or

**TOLL FACILITY RESPONSIBILITY MATRIX
SH 71 Project**

DRAFT

Responsibility Assignment Legend							
Primary Responsibility: P	Support Responsibility: S		Coordination Responsibility Only: C			No Responsibility: N	
Element/Task/Component/ Sub-system	DB Contractor (DB)			Systems Integrator (SI)			Comments Other Responsibility/Information
	Design	Procure	Install/ Construct	Design	Procure	Install / Construct	
							installed in the pavement to the electronics in the cabinets.
In-Pavement Sensors/Loops	N	N	S	P	P	P	SI to saw cut pavement, procure, install, and seal pavement sensors with approved sealant. DB to assure ferrous objects (i.e. rebar, grates, etc.) are not in toll revenue collection detection system(s) zone of influence.
Video Capture Sub-System (VCS/VES) Cameras, Illumination, Sensors and Servers	N	N	S	P	P	P	SI to provide, install, terminate all Video Capture Sub-System (VCS/VES) equipment.
In-Lane Processing Servers and Electronics	N	N	N	P	P	P	SI to provide, install, connect, and terminate all electronics in the cabinet and assures proper communications to the devices on the gantry and/or in the pavement.
POWER DISTRIBUTION SUB-SYSTEM							
Metered power service at each location:	P	P	P	C	N	C	SI to provide power requirements and special requirements for construction of utilities near each Toll System. DB to provide and install necessary conduit & junction/pull boxes.
Metered power service at each toll location:	C	N	C	P	P	P	The SI shall provide and install all other wiring, switches, surge protection/suppression, etc. for power from the meter for the Toll System equipment. SI will terminate all power wiring from ATS at Toll System.
Generators & Automatic Transfer Switches (ATS)	S	N	C	P	P	P	SI to provide generators, ATS, generator cabinets, wiring, connect and terminate all power at the Toll System sites.
Generator Power Source is Natural Gas (if applicable)	P	P	P	S	N	C	If natural gas is available, the DB shall provide, install and incorporate the gas lines into the roadway design. SI to coordinate and provide generator requirements including location for gas feed.
Generator Power Source is propane or diesel	S	N	C	P	P	P	The SI shall provide, and install the propane/diesel tank for the generator if natural gas is not a viable option for the project. The Mobility Authority will

**TOLL FACILITY RESPONSIBILITY MATRIX
SH 71 Project**

DRAFT

Responsibility Assignment Legend							
Primary Responsibility: P	Support Responsibility: S		Coordination Responsibility Only: C			No Responsibility: N	
Element/Task/Component/ Sub-system	DB Contractor (DB)			Systems Integrator (SI)			Comments Other Responsibility/Information
	Design	Procure	Install/ Construct	Design	Procure	Install / Construct	
							decide if propane or diesel will be used.
Uninterruptible Power Supplies (UPS)	S	N	C	P	P	P	SI to provide and install Uninterruptible Power Supply Systems (UPS) in the cabinets. UPS will be required for the Toll System.
Lightning Protection & Grounding	P	P	P	S	C	C	SI to provide specific requirements for equipment lightning protection and grounding. DB to furnish and install required lightning protection and grounding.
COMMUNICATIONS SUB-SYSTEMS							
Conduits/Ducts & Junction/Pull Boxes/Outlets	P	P	P	S	C	S	SI to provide specific Communications design requirements including location of long-radius sweep conduit bends. DB to incorporate into the roadway design and install including conduits, junction boxes, bell ends with pull strings. The DB Contractor shall verify that all duct banks and conduits are clear and have pull strings prior to the beginning of the Toll System installation.
Fiber Optic cabling in conduits for Toll System	S	S	S	P	P	P	SI to provide fiber requirements for Toll System. DB to incorporate into design of backbone and laterals. SI to furnish and install along the corridor from communication hub to cabinets.
Toll Hardware in Cabinets	C	N	C	P	P	P	SI to provide and install all toll hardware within the cabinets. Equipment must be installed in a clean and organized manner and must not be affected by the environmental controls. The SI must provide and install the redundant environmental controls.
Routers	C	N	C	P	P	P	SI to provide, install and configure the routers for connection from hub locations to the Mobility Authority's Traffic Management Center (TMC).
Hubs	N	N	C	P	P	P	If applicable.
Switches	N	N	C	P	P	P	SI to provide, install and configure the switches for connection from tolling to hub locations.
Firewalls	N	N	C	P	P	P	SI to provide, install and configure the necessary firewall for the toll system.
Patch/Distribution Panels	N	N	C	P	P	P	SI to provide and install all the necessary patch and distribution panels to provide Fault Tolerant Single

**TOLL FACILITY RESPONSIBILITY MATRIX
SH 71 Project**

DRAFT

Responsibility Assignment Legend							
Primary Responsibility: P	Support Responsibility: S		Coordination Responsibility Only: C			No Responsibility: N	
Element/Task/Component/ Sub-system	DB Contractor (DB)			Systems Integrator (SI)			Comments Other Responsibility/Information
	Design	Procure	Install/ Construct	Design	Procure	Install / Construct	
							Mode Fiber Optic IP-Based Communication System.
Corridor Communications System	S	N	C	P	P	P	SI to provide Fault Tolerant Single Mode Fiber Optic IP-Based Communication System for Toll Revenue Collection Systems.
Corridor Communications Conduits	P	P	P	C	N	S	DB to provide branch conduit to the TxDOT ductbank system, including conduit, ground boxes, and terminations
Corridor to Traffic Management Center (TMC)	N	N	N	P	P	P	SI to provide Fault Tolerant IP-Based Communication System to the TMC for Toll Revenue Collection Systems.
Data/Communications Service to each Tolling Location	N	N	N	P	P	P	SI to provide system design plans indicating power and communications/data requirements. SI to install up to the Toll System locations at demark panel.
SYSTEMS SERVERS AND SPACE							
Toll Collection Systems Computer(s)	N	N	N	P	P	P	
Support Equipment at CTRMA Offices	N	N	N	P	P	P	SI to provide data and power wiring schematics, equipment rack/cabinet requirement, and elevations, layouts, floor plans, air flow diagrams, and environmental controls load calculations, electrical power distribution, including grounding, bonding, lightning protection, panel boards, TVSS, circuit breakers conduit, conductors, j-boxes, receptacles.
Systems Servers & Workstations	N	N	C	P	P	P	SI to provide, install and configure all system servers and workstations required at the TMC to support the operations and management of the Project.

**TOLL FACILITY RESPONSIBILITY MATRIX
SH 71 Project**

DRAFT

Responsibility Assignment Legend							
Primary Responsibility: P	Support Responsibility: S		Coordination Responsibility Only: C			No Responsibility: N	
Element/Task/Component/ Sub-system	DB Contractor (DB)			Systems Integrator (SI)			Comments Other Responsibility/Information
	Design	Procure	Install/ Construct	Design	Procure	Install/ Construct	
Federal Communication Commission License Preparation and Submission	C	N	N	P	P	P	SI to provide all information necessary to acquire FCC Licensing to the Mobility Authority.
DUCT BANK & INTELLIGENT TRANSPORTATION SYSTEMS (ITS) – TXDOT OWNED							
Duct Bank Adjustment & ITS relocations design	P	P	P	N	N	N	DB is responsible for the design of any necessary ITS relocations, including, foundations, conduits, electrical services, grounding circuits, and support structures. DB responsible for adjusting any existing duct bank manholes and providing new junction/boxes and manholes if in conflict with the project. Coordination with TxDOT will be required.
Duct Bank Adjustments/new connections	P	P	P	S	N	C	DB is responsible for all manhole adjustments and new manhole ties.
Fiber optic cables	P	P	P	N	N	N	Any adjustments to existing cables are DB responsibility.
Relocation of existing CCTV & DMS foundations, conduits, grounding, camera poles, and electrical services	P	P	P	N	N	N	DB is responsible for relocating any existing CCTV and DMS structures and services impacted by the Project Design, including communications and power. Damaged or inoperable equipment shall be moved but not repaired.
Existing and new vehicle detector foundations, conduits, loops, grounding, vehicle detector support structures, and electrical services	P	P	P	N	N	N	DB to coordinate with TxDOT regarding any existing vehicle detector/ loops within the pavement to determine if they need to be replaced/relocated. The DB will replace/relocate any detectors/loops unless TxDOT prefers to do the work. Any damaged detectors/loops that are to remain must be replaced by the DB.
Vehicle detectors, communications, and equipment enclosures	P	P	P	N	N	N	



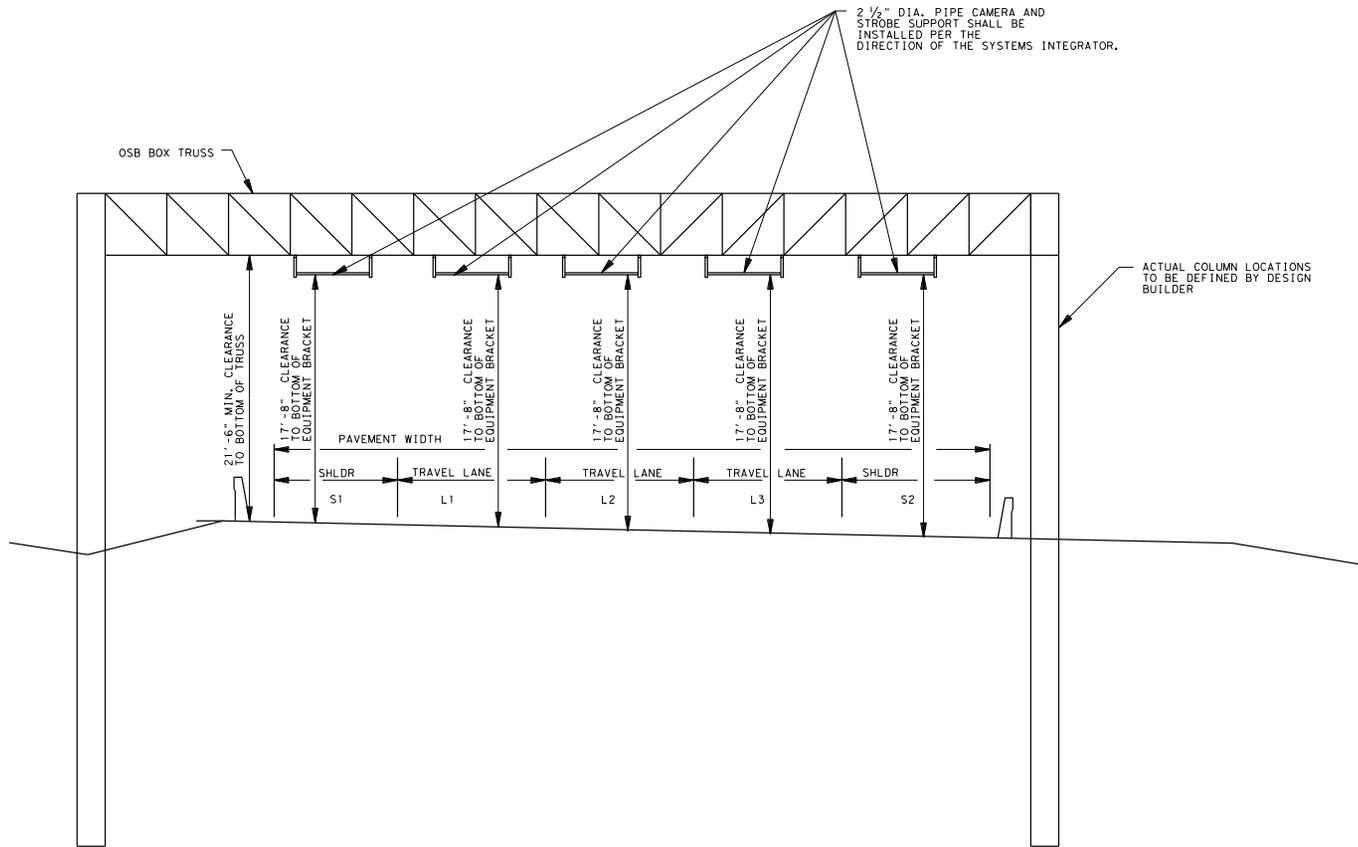
**CENTRAL TEXAS
Regional Mobility Authority**

FIXED PRICE TOLLING STANDARDS
2 - 4 LANES

ISSUED: NOVEMBER 2013

ATTACHMENT D
FIXED PRICE TOLLING STANDARDS

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 Date: 11/15/2013



NOTES

- TWO (2) BRACKETS ARE REQUIRED FOR EACH LOCATION SHOWN. BRACKETS SHALL BE ADJUSTABLE TO ALLOW FOR POSITIONING IN ACCORDANCE WITH THE SYSTEMS INTEGRATOR'S SPECIFICATIONS. DESIGN BUILDER MUST COORDINATE INITIAL PLACEMENT LOCATION WITH THE SYSTEMS INTEGRATOR.
- WALKWAYS ARE REQUIRED TO BE INSTALLED ON ALL GANTRY TRUSSES. LADDERS ARE REQUIRED TO BE INSTALLED ON ALL GANTRY COLUMNS.
- FOR 2 MAIN LANES, REMOVE L3. FOR 4 MAIN LANES, ADD L4.

QUANTITY OF BRACKETS:
 2 LANES = 16 BRACKETS
 3 LANES = 20 BRACKETS
 4 LANES = 24 BRACKETS

INTERIM REVIEW ONLY
 Document incomplete; not intended for permit, bidding or construction.
 Engineer: KRIS Z. KEITH
 P.E. Serial No.: 93753
 Date: 15-NOV-2013

NOT TO SCALE

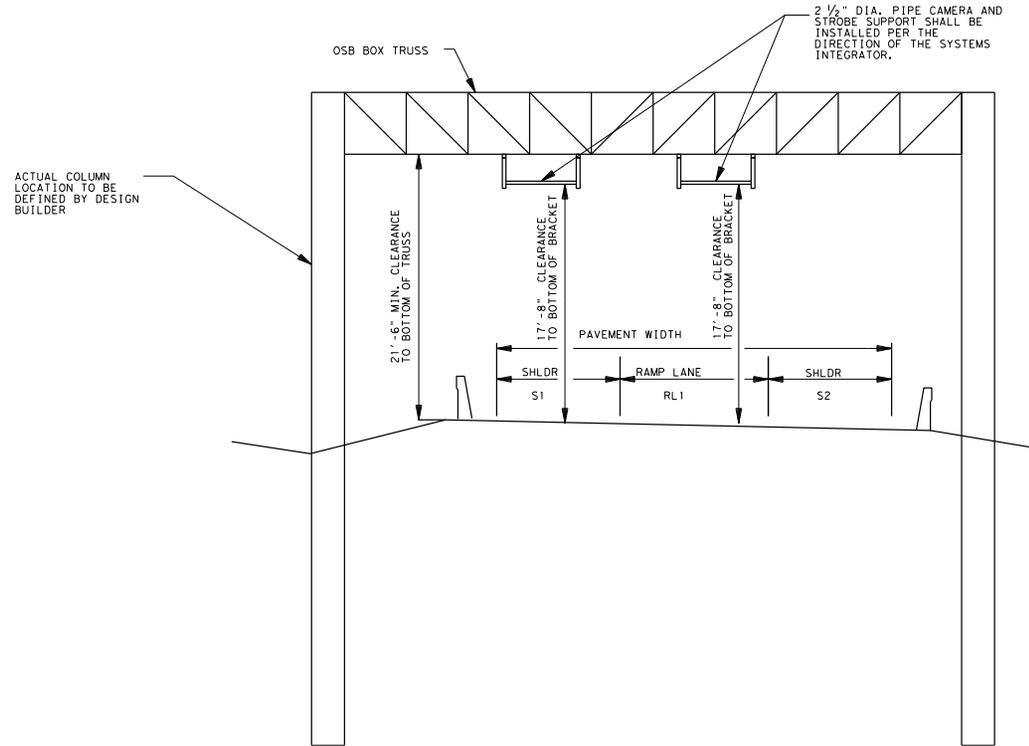
HNTB HNTB Corporation
 The HNTB Companies
 Engineers Architects Planners
 TYPE FIRM REGISTRATION NO. 420

CENTRAL TEXAS
 Regional Mobility Authority

FIXED PRICE
 TOLLING STANDARDS
 MAINLANE GANTRY CROSS-LANE
 TANGENT ELEVATION VIEW

DESIGNED BY: GTO/RO		FEDERAL AID PROJECT NO.		SHEET NO.
6				15
DRAWN BY:	STATE	DIST.	COUNTY	
DR	TEXAS	AUS		
CHECKED BY:	CONT.	SECT.	JOB	HIGHWAY NO.
KK				

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 Scale: 1:10



NOTES

- TWO (2) BRACKETS ARE REQUIRED FOR EACH LOCATION SHOWN. BRACKETS SHALL BE ADJUSTABLE TO ALLOW FOR POSITIONING IN ACCORDANCE WITH THE SYSTEMS INTEGRATOR'S SPECIFICATIONS. DESIGN BUILDER MUST COORDINATE INITIAL PLACEMENT LOCATION WITH THE SYSTEMS INTEGRATOR.
- WALKWAYS ARE REQUIRED TO BE INSTALLED ON ALL GANTRY TRUSSES. LADDERS ARE REQUIRED TO BE INSTALLED ON ALL GANTRY COLUMNS.

QUANTITY OF BRACKETS:
1 RAMP LANE = 8 BRACKETS

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 Engineer: KRIS Z. KEITH
 P.E. Serial No.: 93753
 Date: 15-NOV-2013

NOT TO SCALE

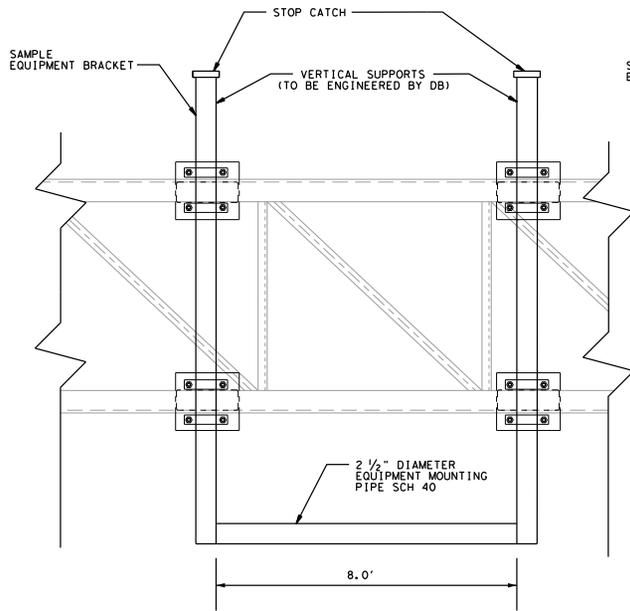
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 Regional Mobility Authority

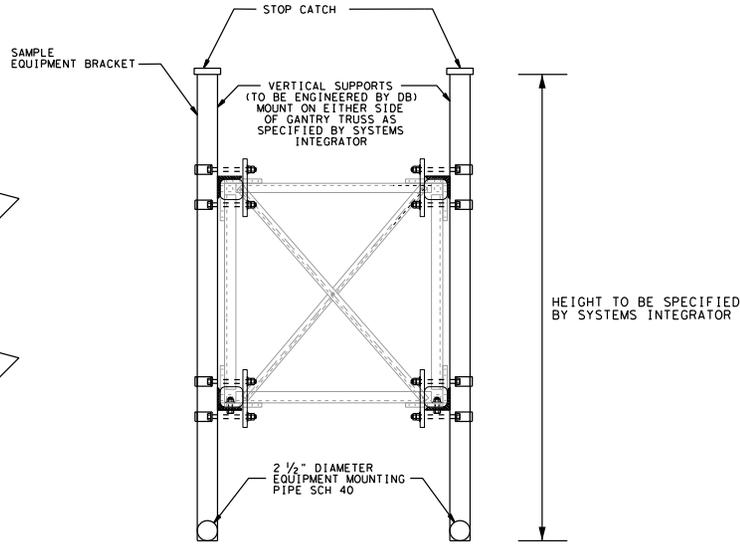
**FIXED PRICE
 TOLLING STANDARDS
 RAMP CROSS-LANE
 TANGENT ELEVATION VIEW**

DESIGNED BY: GTO, RBO		FEDERAL AID PROJECT NO.		SHEET NO.
6				16
DRAWN BY:	STATE	DIST.	COUNTY	
DR	TEXAS	AUS		
CHECKED BY:	CONT.	SECT.	JOB	HIGHWAY NO.
KK				

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ELEVATION TOLL GANTRY BRACKET



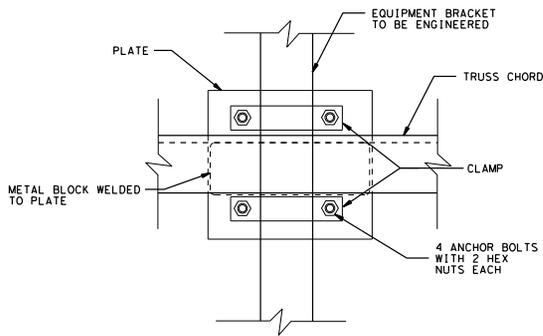
CROSS SECTION OF TOLL GANTRY BRACKET

GENERAL NOTES:

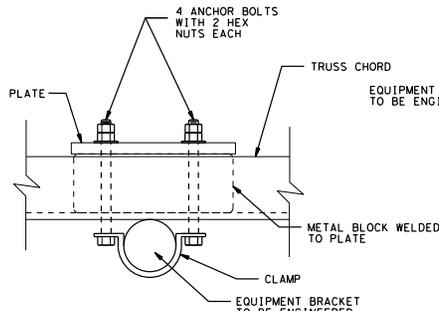
- 1) THESE DETAILS ARE FOR USE WITH TXDOT STANDARD OVERHEAD SIGN BRIDGE (OSB) OR CANTILEVER OVERHEAD SIGN SUPPORT (COSS).
- 2) FOR CONNECTION DETAILS NOT SHOWN SEE OVERHEAD SIGN BRIDGE TRUSS DETAILS (OSBC) OR CANTILEVER OVERHEAD SIGN SUPPORT DETAILS (COSSD).
- 3) MATERIALS SHALL BE COMPATIBLE WITH THE REQUIREMENTS OF THE ASSOCIATED REFERENCE SIGN BRIDGE STANDARDS.
- 4) QUANTITY OF EQUIPMENT BRACKETS AND LOCATION TO BE SPECIFIED BY SYSTEM INTEGRATOR.
- 5) DESIGN OF EQUIPMENT BRACKET AND CONNECTION TO OVERHEAD TRUSS TO BE PROVIDED BY DESIGN BUILDER. AVI ANTENNA AND CAMERA MOUNTING HARDWARE AND CONNECTION DETAILS TO BE PROVIDED BY SYSTEM INTEGRATOR.
- 6) SHOP DRAWINGS FOR AVI ANTENNA AND VES CAMERA SUPPORT SHALL BE PROVIDED FOR ENGINEER'S REVIEW.
- 7) DESIGNER BUILDER TO SPECIFY APPROPRIATE SPAN, ICE LOADING, AND WIND ZONE. TRUSS DYNAMIC RESPONSE AND DEFLECTION SHALL BE COMPATIBLE WITH TOLL SYSTEM REQUIREMENTS.
- 8) DESIGN OF OSB AND COSS COLUMNS, DRILLED SHAFTS AND TRUSS TO COLUMN CONNECTION TO BE PROVIDED BY DESIGN BUILDER.
- 9) EQUIPMENT BRACKET AND ALL MOUNTING HARDWARE SHALL BE HOT DIP GALVANIZED.

INTERIM REVIEW ONLY
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 Engineer: KRIS Z. KEITH
 P.E. Serial No.: 93753
 Date: 15-NOV-2013

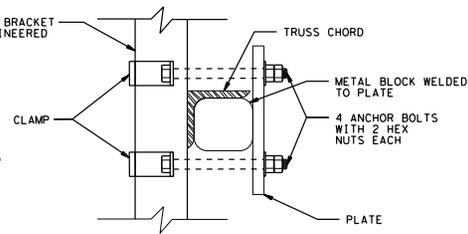
NOT TO SCALE



FRONT VIEW
(TOP CHORD SHOWN,
FLIP HORIZ. FOR
BOTTOM CHORD)



TOP VIEW
(TOP CHORD SHOWN,
BOTTOM VIEW FOR
BOTTOM CHORD)



SIDE VIEW
(TOP CHORD SHOWN,
FLIP HORIZ. FOR
BOTTOM CHORD)

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 Regional Mobility Authority

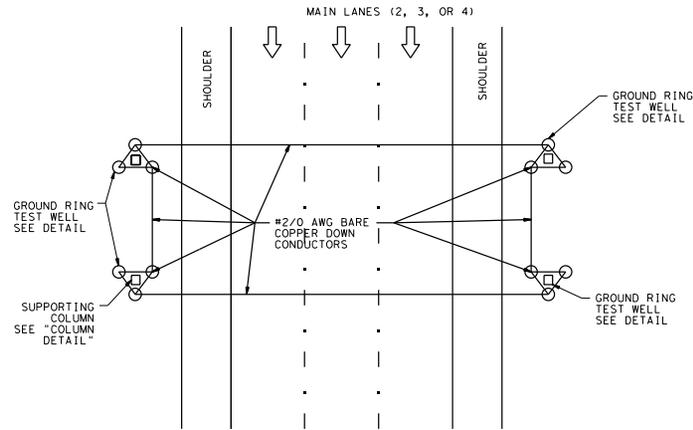
FIXED PRICE
 TOLLING STANDARDS
 TOLL GANTRY
 MISCELLANEOUS
 DETAILS

MG-1

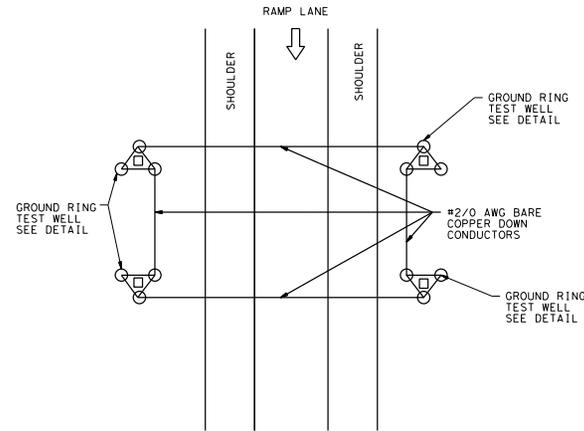
DESIGNED BY:	STO. NO.	FEDERAL AID PROJECT NO.	SHEET NO.
	6		17
DRAWN BY:	STATE	DIST.	COUNTY
DR	TEXAS	AUS	
CHECKED BY:	CONT.	SECT.	JOB
KK			HIGHWAY NO.

Scale: 1:20

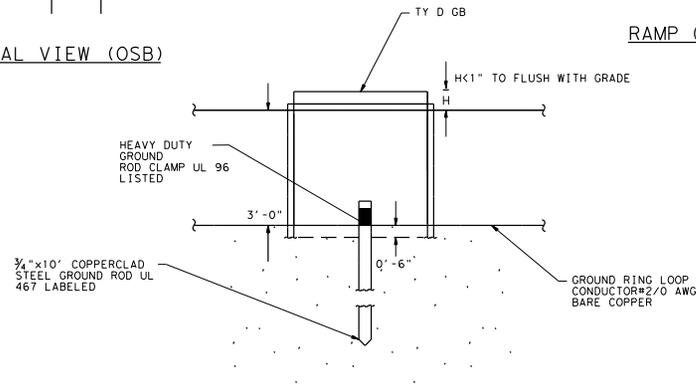
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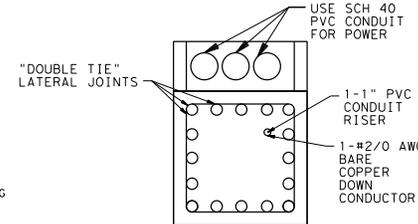
MAINLANE GROUND RING AERIAL VIEW (OSB)



RAMP GROUND RING AERIAL VIEW (OSB)



GROUND RING TEST WELL DETAIL

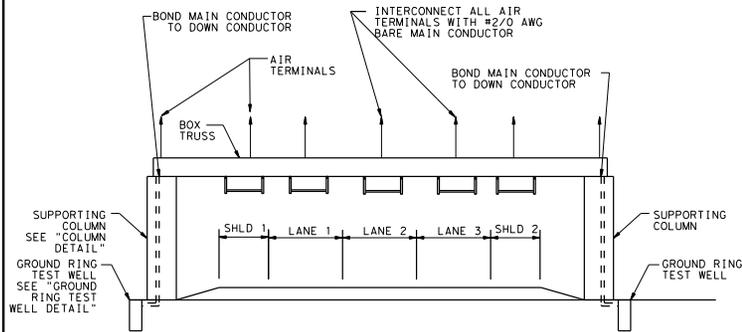


CONCRETE COLUMN DETAIL

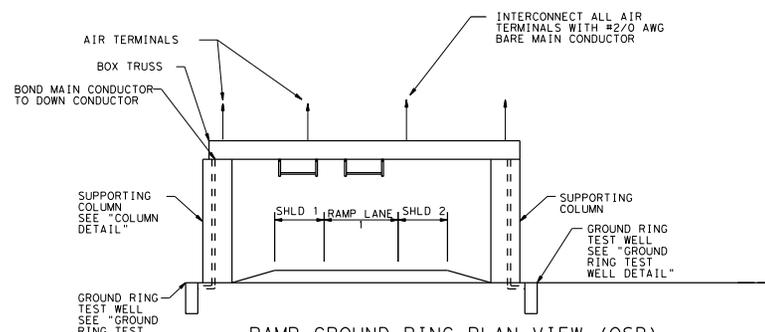
- NOTES:
- 1) LIGHTNING PROTECTION SYSTEM DESIGN (LPS) IS PROVIDED FOR INFORMATION ONLY. ULTIMATE LPS DESIGN SHALL COMPLY WITH NFPA 780 AND CURRENT NATIONAL ELECTRIC CODE (NEC).
 - 2) ALL STRUCTURAL CONCRETE AND CONDUIT INSTALLATIONS SHALL COMPLY WITH ACI 318 CHAPTER 6.
 - 3) LPS SHALL BE INSTALLED BY A UL LISTED INSTALLER.
 - 4) LPS SHALL BE UL MASTER LABEL CERTIFIED.
 - 5) ALL LPS MATERIALS SHALL MEET NFPA 780 CLASS II REQUIREMENTS.
 - 6) ALL REBAR LATERAL ELEMENTS, LAP JOINTS, AND CONNECTIONS SHALL BE "DOUBLE TIED".
 - 7) FOR DETAILS OF TOLL COLLECTION SYSTEMS CONFIGURATION, COORDINATE WITH SYSTEM INTEGRATOR.
 - 8) AIR TERMINALS SHALL ACCOMMODATE A GUIDE EVERY 14'-0" CENTERED ON GANTRY TRUSS.
 - 9) FOR 2 MAIN LANES, REMOVE LANE 3. FOR 4 MAIN LANES, ADD LANE 4.

INTERIM REVIEW ONLY
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 Engineer: FABIAN KALAPACH
 P.E. Serial No.: 58100
 Date: 15-NOV-2013

NOT TO SCALE



MAINLANE GROUND RING PLAN VIEW (OSB)



RAMP GROUND RING PLAN VIEW (OSB)

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 Engineers Architects Planners
 TYPE FIRM REGISTRATION NO. 420

CENTRAL TEXAS
 Regional Mobility Authority

FIXED PRICE
 TOLLING STANDARDS
 LIGHTNING PROTECTION
 SYSTEM DETAILS

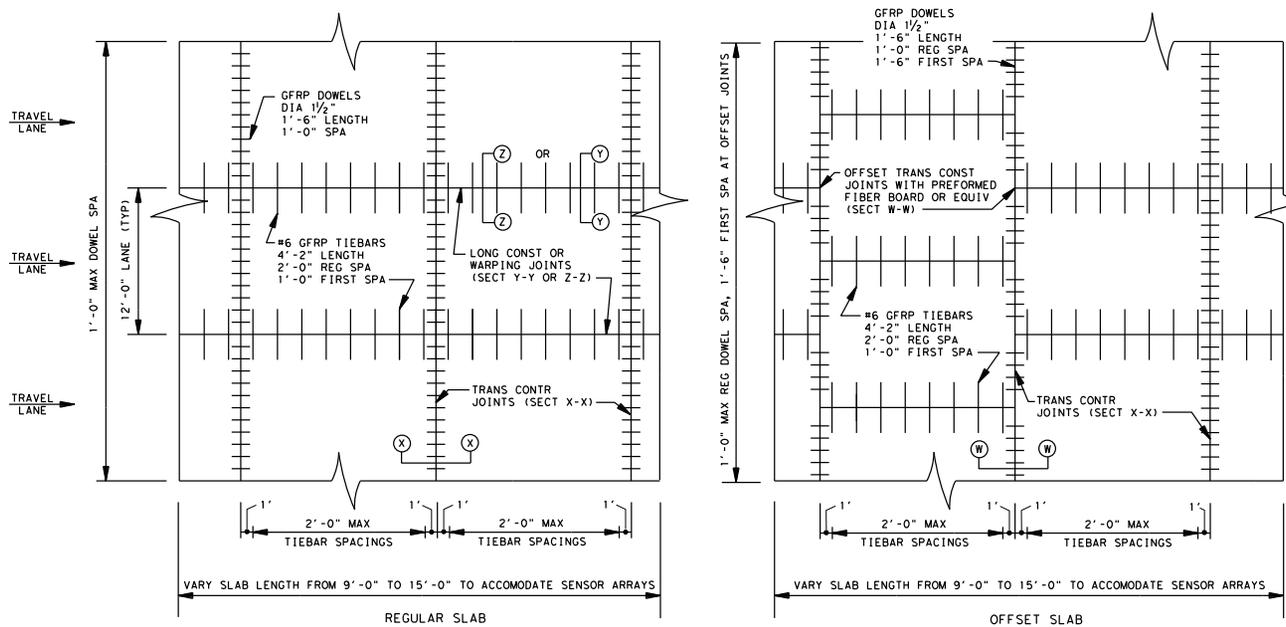
LP-1

DESIGNED BY:	STO. NO.	FEDERAL AID PROJECT NO.	SHEET NO.
6			18
DRAWN BY:	STATE	DIST.	COUNTY
DR	TEXAS	AUS	
CHECKED BY:	CONT.	SECT.	JOB
KK			HIGHWAY NO.

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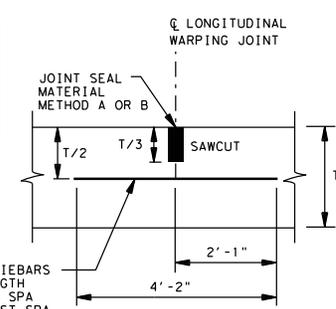
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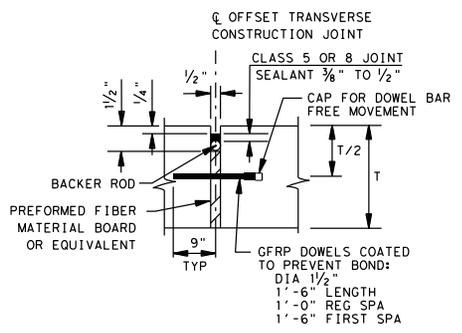


JOINTED PAVEMENT DETAIL LAYOUT
 NOT TO SCALE

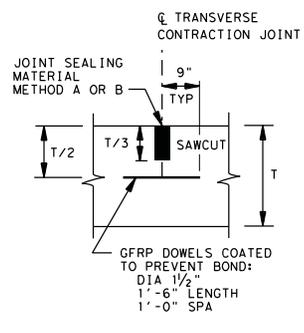
- GENERAL NOTES**
1. THE TERMINAL ANCHOR SLAB DETAILS ARE AS SHOWN ELSEWHERE IN THE PLANS.
 2. DETAILS FOR PAVEMENT SLAB WIDTH, THICKNESS, AND CROWN CROSS-SLOPE ARE SHOWN ELSEWHERE ON THE PLANS.
 3. THE DETAIL FOR THE JOINT SEALANT AND RESERVOIR SHALL BE AS SHOWN ON "CONCRETE PAVEMENT DETAIL, JOINT SEALS" STANDARD (JS-94).
 4. FOR FURTHER INFORMATION REGARDING THE DETAIL OF CONCRETE AND REINFORCEMENT, REFER TO THE GOVERNING SPECIFICATIONS FOR "CONCRETE PAVEMENT" AND "GFRP REINFORCEMENT".
 5. PAVEMENT WIDTH OF MORE THAN 16' SHALL HAVE A LONGITUDINAL JOINT (SECTION Z-Z OR Y-Y). THESE JOINTS SHALL BE LOCATED WITHIN 6" OF THE LANE LINE UNLESS THE JOINT LOCATION IS SHOWN ELSEWHERE ON THE PLANS.
 6. SAW CUT DEPTH FOR LONGITUDINAL AND TRANSVERSE CONTRACTION JOINTS MAY BE ONE FOURTH THE SLAB THICKNESS WHEN CRUSHED LIMESTONE IS USED AS THE COARSE AGGREGATE.
 7. CONCRETE SLABS WIDER THAN 100' WITHOUT A FREE JOINT, ARE NOT COVERED BY THIS STANDARD.
 8. SLAB LENGTHS SHOWN FOR 12' LANE. LONGITUDINAL AND TRANSVERSE SLAB DIMENSIONS FOR OTHER LANE WIDTHS MUST NOT VARY MORE THAN 25% FROM EACH OTHER.



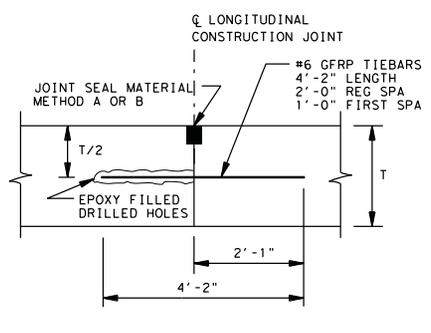
INTERIM REVIEW ONLY
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 Engineer: IAN ANDREW FRASER
 P.E. Serial No.: 88483
 Date: 15-NOV-2013



OFFSET TRANSVERSE CONSTRUCTION JOINT
 SECTION W-W



TRANSVERSE CONTRACTION JOINT
 SECTION X-X



LONGITUDINAL CONSTRUCTION JOINT
 SECTION Y-Y

LONGITUDINAL WARPING JOINT
 SECTION Z-Z

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 Engineers Architects Planners
 TYPE FIRM REGISTRATION NO. 420

CENTRAL TEXAS
 Regional Mobility Authority

**FIXED PRICE
 TOLLING STANDARDS**
 JOINTED CONCRETE
 PAVEMENT
 JC-1

DESIGNED BY:	STO. NO.	FEDERAL AID PROJECT NO.	SHEET NO.
DR	6		24
CHECKED BY:	STATE	DIST.	COUNTY
KK	TEXAS	AUS	
	CONT.	SECT.	JOB
			HIGHWAY NO.

ATTACHMENT E

PRICE SHEET

SH-71

Toll System Installation/Integration

Task No.	Description	Qty	Unit	Unit Price (US \$'s)	Extended Price (US \$'s)	Equipment / Subcontractors	Labor	Markup on Sub's & Equipment	%	Total	Total Hours
1	HW - Materials / Equipment	1	Lot	495,840.11	495,840.11	413,200	222,566	82,640	20.0%	495,840	1,187
2	Program Management	1	Lot	222,566.37	222,566.37		129,354			222,566	860
3	SW Design & System Documentation	1	Lot	129,354.18	129,354.18		71,341			129,354	480
4	SW Development	1	Lot	71,340.92	71,340.92		182,348			182,348	1,220
5	SW Integration/Test (Commissioning, Final Accept, etc.	1	Lot	182,348.02	182,348.02	115,500	167,368	23,100	20.0%	305,968	1,394
6	Installation	1	Lot	305,968.26	305,968.26	115,050	11,292	23,010	20.0%	149,352	73
7	Fiber-1: CTRMA	1	Lot	149,352.13	149,352.13	361,757	42,382	72,351	20.0%	476,490	275
8	Fiber-2: TxDOT	1	Lot	476,490.38	476,490.38						
9	Bonding	1	LS	26,234.13	26,234.13	26,234				26,234	
TOTAL											5,490
						1,031,741	826,652	201,101		2,059,495	

The Pricing shown above Excludes:

- All Recurring Data Communication Costs
- Recurring 3rd-Party SW/HW Support Agreements & SW Licenses
- Spares Replenishment Costs
- Excludes System HW/SW Warranty/Maintenance Services & Support
- Excludes MOT for Toll System Installation (Includes MOT for Fiber Installation)

DRAFT

Labor Rates

	Partial CY2014 yr-4 Esc
Position Title	2.5%
Software Engineer	\$139.62
System Engineer	\$152.86
Technician	\$107.12
Database Administrator	\$198.59
Documentation Clerk	\$143.23
Testing Engineer	\$151.65
Network Engineer	\$138.41
Project Manager	\$198.59
Blended PW Technician Rate	\$126.25

SW Integration/Test

	Total Hrs
Database Administrator	
Database Developer	200
Software Engineer - Image Processing	-
Software Engineer - Real Time	160
Software Manager	120
Software Programmer - Web/Middle Tier	200
Documentation / Technical Writer / CAD	
Project Manger	80
System Engineer	40
Test / QA Manager	80
Test Engineer	320
Field Tech Team Lead / Supervisor	
Field Technician	
Installation/Field Manager	
Client Account Manager (Maint)	
Network / Systems Administrator	120
Software Support (Maint)	

Notes: Includes the set up of the machines, configuration and testing of the network, set up of the reports, commission test, and operational test.

CTRMA/TxDOT FIBER INSTALLATION SH 71

ITEM CODE	DESCRIPTION	UNIT	Total	
			CTRMA	TxDOT
610-2031	Comm Cable (6 Pair) (22AWG)	LF	0	31300
620-2018	Elec Condr (No 14) Insulated	LF	6000	9300
618-2022	Conduit PVC (3")	LF	440	0
6014-2011	Fiber Optic Cable (Single-Mode)(12 Fiber)	LF	710	63660
6014-2014	Fiber Optic Cable (Single-Mode)(48 Fiber)	LF	18125	16400
6014-2020	Fiber Optic Splice Enclosure	EA	1	0
6014-2021	Fiber Optic Patch (12 Position)	EA	0	14
6014-20XX	Fibr Patch Panel (24 Position)	EA	1	1
6014-2022	Fiber Patch Panel (48 Position)	EA	0	0
6014-20XX	Fiber Patch Panel (72 Position)	EA	0	2
	SFP (SM)	EA	6	6
	Interconnect Cabinet	EA	1	0
	Cabinet Foundation	CY	1	0

Assumptions:

1. The fiber design for TxDOT is based on the Austin District's current method of ITS installation which provides a dedicated fiber to each device.
 - a. TxDOT does not allow mid-entry splices to their fiber trunk
 - b. Because of (a.) - A new Communications Hub building is needed to integrated all fiber runs to one central location on the corridor
 - c. The new Comm Hub building for this estimated is located at the underpass of Sprit of Texas and SH-71.
 - d. The estimate does not include the cost of procuring or installing a new Comm Hub building, it considers this design item as a D/B provided item.
 - e. Each CCTV camera has a dedicated fiber cable and a dedicated twisted pair comm. cable from the Camera to the single Hub building
 - f. Each DMS has a dedicated fiber cable from the DMS to the single Hub building.
2. An Interface cabinet for interconnecting the TxDOT trunk and the CTRMA trunk along SH-71 to the proposed CTRMA/TxDOT shared fiber trunk along SH-130 is included in the estimate under CTRMA costs.
3. TxDOT and CTRMA will have separate trunks along SH-71.

ATTACHMENT F

PRELIMINARY PROJECT SCHEDULE AND MILESTONES

State Highway 71 Toll Lanes

(Dates and Durations Subject to Change)

Task	Duration and/or Milestone Date
Design-Build Contract Executed	August 2014
Construction Duration (Approximate)	2 years
Open to Traffic	Winter 2016



**CENTRAL TEXAS
Regional Mobility Authority**

AGENDA ITEM #12 SUMMARY

Approve a resolution supporting the proposed amendment to the Texas Constitution providing for increased state funding for roads, highways and bridges.

Strategic Plan Relevance:	Regional Mobility
Department:	Executive Director
Associated Costs:	N/ A Briefing Only
Funding Source:	N/ A
Board Action Required:	Yes

Description of Matter: The 83rd Texas Legislature passed SJR 1 proposing a constitutional amendment to be placed on the November 4, 2014 state ballot. The ballot language provides:

“The constitutional amendment providing for the use and dedication of certain money transferred to the state highway fund to assist in the completion of transportation construction, maintenance, and rehabilitation projects, not to include toll roads;”

The proposed amendment would result in an estimated \$1.4 billion per year for the state highway fund. With the passage of this resolution, the Mobility Authority would join other groups and regional mobility authorities who support this amendment to provide additional funding to the state highway fund.

Reference documentation:	Draft Resolution
Contact for further information:	Mike Heiligenstein, Executive Director Brian Cassidy, Outside Counsel

**GENERAL MEETING OF THE BOARD OF DIRECTORS
OF THE
CENTRAL TEXAS REGIONAL MOBILITY AUTHORITY**

RESOLUTION NO. 14 - _____

**SUPPORTING THE PROPOSED AMENDMENT TO THE
TEXAS CONSTITUTION PROVIDING FOR INCREASED STATE FUNDING
FOR ROADS, HIGHWAYS AND BRIDGES.**

WHEREAS, the 83rd Texas Legislature passed SJR 1 proposing a constitutional amendment to be placed on the November 4, 2014 state ballot affecting the state's roads, highways, and bridges; and

WHEREAS, SJR 1 prescribes the ballot language as follows: *"The constitutional amendment providing for the use and dedication of certain money transferred to the state highway fund to assist in the completion of transportation construction, maintenance, and rehabilitation projects, not to include toll roads;"* and

WHEREAS, the proposed amendment will aid in maintaining the current infrastructure and will ease congestion by funding new projects for added capacity without any new or increased taxes, fees or debt; and

WHEREAS, a strong transportation system is fundamental to Texas' quality of life and economic vitality, and the state's ability to attract new businesses and generate new jobs; and

WHEREAS, the proposed amendment would result in an estimated \$1.4 billion per year for the state highway fund; and

WHEREAS, while the proposed amendment would not fully fund the state's overall transportation needs, it would be an important step toward securing critical funding for transportation projects in Texas;

NOW THEREFORE, BE IT RESOLVED, that the Central Texas Regional Mobility Authority Board of Directors supports the passage of the proposed constitutional amendment to help address Texas' transportation infrastructure needs.

Adopted by the Board of Directors of the Central Texas Regional Mobility Authority on the 30th day of July, 2014.

Submitted and reviewed by:

Approved:

Andrew Martin
General Counsel for the Central
Texas Regional Mobility Authority

Ray A. Wilkerson
Chairman, Board of Directors
Resolution Number: 14-_____
Date Passed: 7/30/2014



**CENTRAL TEXAS
Regional Mobility Authority**

AGENDA ITEM #13 SUMMARY

Quarterly briefing on the MoPac Improvement Project.

Strategic Plan Relevance: Regional Mobility

Department: Engineering

Associated Costs: N/A Briefing Only

Funding Source: N/A

Board Action Required: No

Description of Matter:

The report is an account of the activities on the MoPac Improvement Project from April through June, 2014.

Reference documentation:

GEC Quarterly Activities Report and Board Presentation

Contact for further information:

Wesley M. Burford, P.E., Director of Engineering



◀◀ MOPAC
IMPROVEMENT
PROJECT ▶▶

QUARTERLY
PROGRESS
REPORT

No. 15 | July 2014





CENTRAL TEXAS
Regional Mobility Authority

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CH2MHILL®

PROJECT PARTNERS





QUARTERLY PROGRESS REPORT No. 15

INTRODUCTION

The MoPac Improvement Project is a \$204 million project which will add one Express Lane in each direction along an approximately 11-mile stretch of MoPac from Cesar Chavez Street in downtown Austin to Parmer Lane north of Austin within existing right of way. The Project is an effort to address the mobility problem in this corridor and takes into account the needs of drivers, transit riders, pedestrians, bicyclists, and the concerns of surrounding neighbors.

Environmentally cleared in August 2012, the Project will be built in cooperation with the Texas Department of Transportation (TxDOT), the Capital Area Metropolitan Planning Organization (CAMPO), the City of Austin, Capital Metro (CapMetro), and the Federal Highway Administration (FHWA).

The Express Lanes will be located in the middle of the MoPac corridor separated from the existing general purpose lanes by a four foot wide striped buffer zone with flexible pylons. Drivers will be able to access the MoPac Express Lanes at Cesar Chavez Street, near Far West Boulevard, or near Parmer Lane. While this addition of lanes will require widening of the pavement, the Express Lanes project requires no property acquisition and all existing non-toll general purpose lanes and the UPRR corridor will remain.

In addition to the Express Lanes, the MoPac Improvement Project will include: sound walls; a Collector/Distributor road under Steck Avenue; aesthetic enhancements and significant landscaping; bicycle and pedestrian improvements including two shared-use paths; full mainlane overlay with porous friction course (PFC) pavement which serves as a wet weather safety improvement measure; enhanced incident management (cameras and traffic data collection); and a significant community relations effort.

The Mobility Authority entered into a contract with CH2M HILL to design and build the MoPac Improvement Project. The agreement requires the project to be substantially complete by September 17, 2015. The Contractor developed an acceptable Baseline CPM (Critical Path Method) Schedule for the Project. The Mobility Authority issued Notice to Proceed (NTP) on April 18, 2013.

This report describes the status of the MoPac Improvement Project and documents the activities accomplished from April through June 2014.

LOCATION MAP



KEY TO LINES

- Express Lane Southbound
- Express Lane Northbound
- ⋯ Union Pacific Railroad
- ⋯ Capital Metro Rail



Segment 1: Parmer to UPRR
Segment 2A: UPRR to US 183
Segment 2B: US 183 to RM 2222
Segment 3: RM 2222 to Enfield
Segment 4: Enfield to Cesar Chavez

SUMMARY OF ACTIVITIES

The following activities have been accomplished by the Mobility Authority, its consultants, and the D/B Contractor during the reporting period.

DESIGN ACTIVITIES

- Weekly task force meetings continue and over the shoulder informal reviews have been ongoing. The Mobility Authority's GEC is performing oversight of the D/B Contractor activities.
- The following were reviewed by Oversight Team and accepted:
 - Segment 4 Northbound Undercrossing 100% Design Plans in April
 - UPRR Pedestrian Bridge 100% Design Plans in April
 - Early Release for Construction (ERFC) Segment 1, 2A in April
 - Release for Construction (RFC) Design for:
 - Windsor Rd SB Bridge in April
 - RM2222 NB Bridge in April
 - Segment 3 Ph 1B in May
 - Segment 4 Ph1, 2 in May
 - Segment 4 Ph 3 in June
 - Segment 4 NB Undercrossing in June
 - UPRR Pedestrian Bridge in June
- Received Water Pollution Abatement Plan (WPAP) approval from TCEQ in April
- Reviewed CO-01A 5th Street Reconfiguration Preliminary Design and Final Design Exception Report in June.
- Numerous design change reviews and Requests for Information.
- Coordination continues with UPRR and CapMetro for work within their ROW.
- Change Order development and design revisions have been ongoing.

CONSTRUCTION ACTIVITIES

- D/B Contractor continued with striping and Metal Beam Guard Fence (MBGF) removals.
- Placement of temporary traffic barrier continued along the corridor.
- Erosion control measures were maintained and new installations were placed for areas commencing construction along the corridor.
- Fabrication of pre-cast concrete barrier rail continues at Tricon in New Braunfels, TX.
- Fabrication of sound wall panels continues in Kerville, TX.
- Installation of drilled shafts for bridges.
- MoPac Corridor Courtesy Patrol is operating as required.
- Roadway maintenance is being performed as required.
- Installation of temporary lighting along the corridor.
- Installation of drainage structures along the corridor.
- Excavation and grading for retaining walls.
- Utility relocations.
- Removals of pavement markings and replacement.

PROJECT PROGRESS

As of June 30, 2014, 49% of the 882 calendar days to scheduled Substantial Completion have expired. The Contractor’s latest schedule indicates the project is maintaining schedule for Substantial Completion on September 12, 2015. However, the GEC team has identified several issues of concern and is working with the Contractor to address these issues to meet the contractual Substantial Completion Date. There are 444 calendar days until Substantial Completion and the Contractor is allowed an additional 120 calendar days for Final Acceptance.

Construction Progress through June 2014

Based on the assessment of the GEC, the summary of the project progress achieved on major work tasks through the end of June 2014 is as follows:

MoPac Improvement Project Progress For the Period Ending June 2014

Design + Construction Tasks	% Complete
Geotech/Survey	100%
Development Design	89%
Environmental Monitoring	29%
Traffic Control/Detours/Temp Barrier	30%
Earthwork/Demolition/Removals/Drainage	13%
Structures/Bridges/Retaining Walls	7%
Sound/Neighborhood Walls	0%
Utilities	10%
Pavement/Subbase/Base Course	0%
Permanent Lighting/Signing/Striping	1%
Toll Facilities	0%
Landscaping/Shared Use Path/Sidewalks	0%

CH2M HILL’s Substantial Completion date per their approved baseline schedule is September 12, 2015.

SCHEDULE OF CONTRACTUAL PROJECT MILESTONES

- **February 27, 2013:** Selection of Best Value Proposer
- **April 18, 2013:** NTP Issued
- **September 17, 2015:** Substantial Completion

- **January 15, 2016:** Final Acceptance

UPCOMING ANTICIPATED PROGRESS ACTIVITIES

July 2014

- UPRR Right of Entry (ROE) Applications and Construction Submittals
- Soundwall 03 100% Review
- Segment 1 and 2 Median Earthwork
- Segments 1 & 2A Plan Acceptance
- Segments 1 & 2A and Phase 1 Construction
- Segment 2B Phase 1 Drainage and Structures Construction
- Segment 3 Sound Wall Shaft Construction
- Segment 3 Drainage Structures
- Segment 3 Duct Bank Relocation
- Segment 3 Overhead Sign Foundations
- Segment 3 Drainage Structures
- Segment 4 Temporary Traffic Control Implementation
- Segment 4 12" Waterline Relocation Segment
- RM 2222 SB, Enfield Bridge, Westover, Cap Metro, and 45th Street Bridge Construction
- Median Widening at US 183 to Enfield
- Jack and Bore for Undercrossing Drainage
- Pre-stressed Beam Fabrication
- Soundwall Panels and Post Fabrication

August 2014

- Set Girders at Enfield
- Set Girders at 2222 SB
- Set Girders at 45th Street
- Segment 3 Utility Relocations
- Sound Wall Construction
- Neighborhood Wall Construction
- Utility Relocation at Enfield
- Outside Widening at US 183 to Enfield
- UPRR Bridge Construction
- Pre-stressed Beam Fabrication

September 2014

- Segments 1 and 2A Begin install of Permanent Precast Barrier
- Far West Toll Equipment Site Construction
- Segment 4 Structural Excavation and Soil Nail Walls
- Westover and Windsor Bridge Substructures
- Water Line Relocate at Cesar Chavez
- RM 2222 NB Bridge Construction
- 45th Street Hardscape

PROGRESS PHOTOS



Sound Wall Mockup Inspection - April 2014



Median Traffic Barrier Demolition near Steck Avenue – April 2014



RM 2222 Bridge Foundation Drilling – April 2014



Retaining Wall Construction in Median at RM 2222 – June 2014

PROJECT FINANCIAL STATUS

All CH2M HILL draw requests are accompanied by an approved progress schedule.

Summary of Project financial status through June 2014

Original CH2M HILL Contract Amount:	\$136,632,100
<i>Authorized Changes (Change Order and/or Amendments):</i>	
Previous Period Change Orders	\$213,733.58
Current Change Order No. 02- Toll Gantry Pavement Requirements	<u>\$0.00</u>
Current Authorized Contract Amount:	\$136,845,833.58
CH2M HILL Payments:	
Amount of Draw Nos. 01-09 (July 2013 – March 2014)	\$24,143,102
Amount of Draw No. 10 (April 2014)	\$2,939,197
Amount of Draw No. 11 (May 2014)	\$1,889,030
Amount of Draw No. 12 (June 2014)	<u>\$2,305,094</u>
Total Requested Amount To-Date through Draw No. 12:	\$31,276,423
Retainage withheld**:	<u> \$0</u>
Approved Amount for Work Completed through Draw No. 12:	\$31,276,423
Total Project Budget Expended Through June 2014:	22.9%
Amount remaining for work to be completed:	\$105,569,410

**Retainage to be withheld only after 95% of the Authorized contract price has been paid.

Summary of Change Orders During Reporting Period

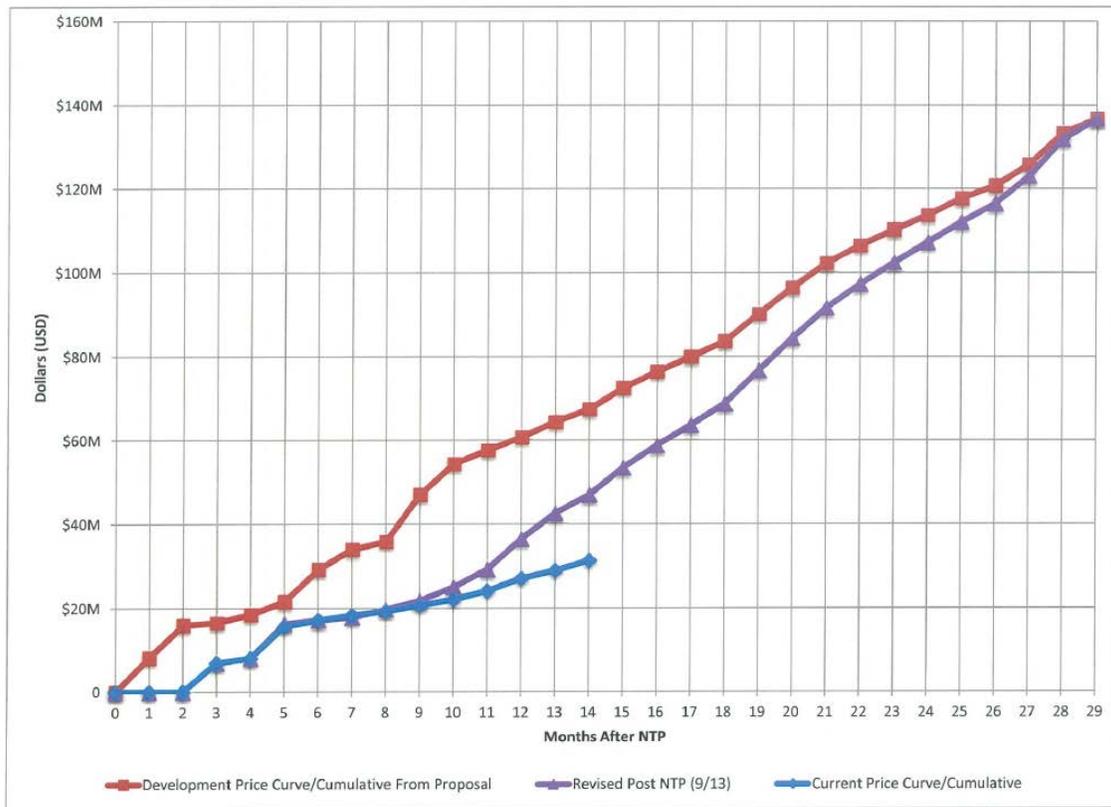
Change Order No. 02 – Toll Gantry Pavement Requirements –This change order is for a change in scope to replace jointed concrete reinforced pavement with asphalt at toll gantry locations. This change order was a \$0 cost change to the scope, submitted February 27, 2014 and executed in March 2014.

Project Cash Flow Curve – March 2014

Cash Flow Curves are provided by CH2M HILL at the time of draw request submittal.



PAYMENT CURVE
COMPARITIVE GRAPH



EMPLOYMENT REPORTING STATUS

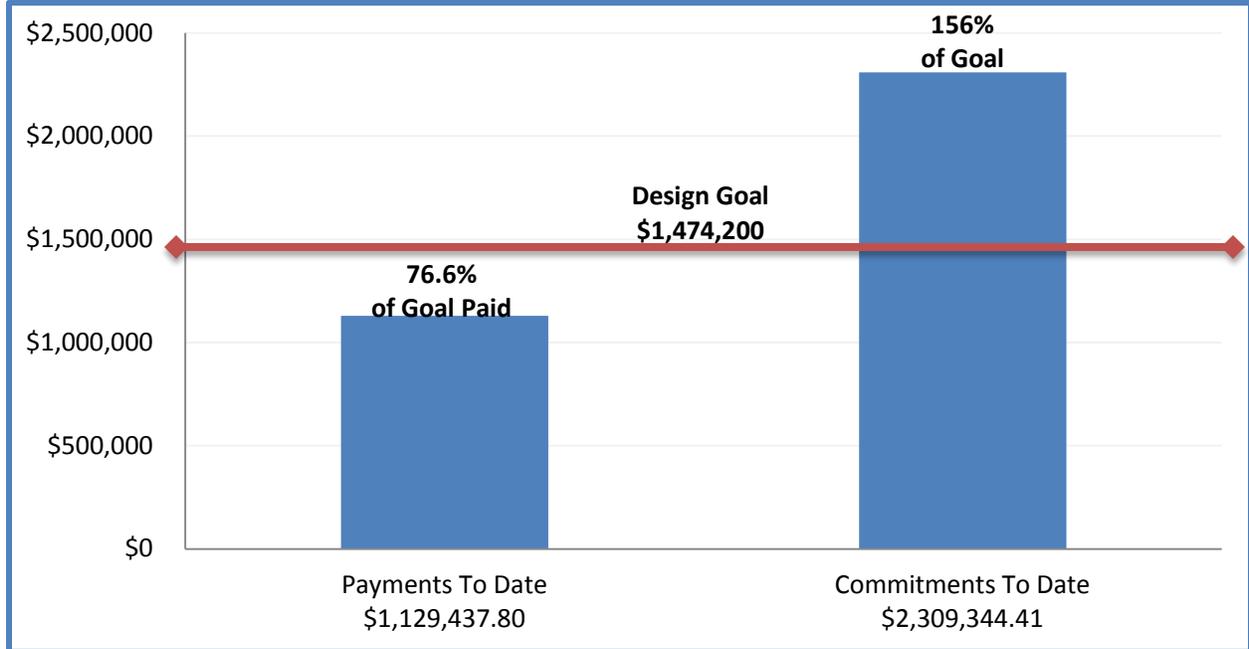
Design and construction of the MoPac Improvement Project is estimated to have supported approximately 203 jobs during the reporting month of June 2014. This estimated number of jobs supported by the project includes: the design personnel and management staff; the construction personnel and their subcontractors; construction management staff, including inspectors and subconsultants; and the general engineering consultant staff and their subconsultants.

DBE STATUS

CH2M HILL plans to meet the Disadvantaged Business Enterprise (DBE) goal of 11.7% for both the design services and the construction work. The overall total DBE requirement for the project, based on the original contract value, is \$15,985,955. The current committed total for all DBE subcontracts is \$17,012,278. The charts below reference the current DBE Commitments vs. Payments to date.

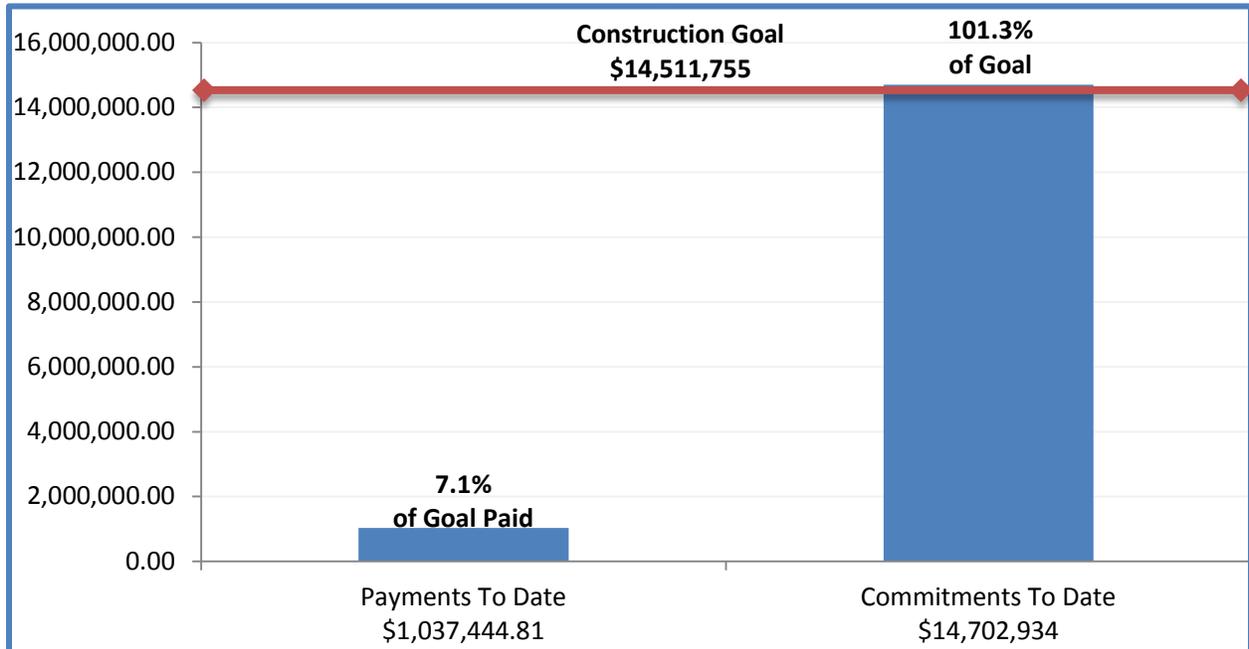
Design

DBE Design Commitment vs. Payment For Period Ending June 2014



Construction

DBE Construction Commitment vs. Payment For Period Ending June 2014



COMMUNITY RELATIONS

- The team staffs a **24/7 Hotline** (via both email and phone), and responds to the steady number of inquiries about the project. Within the reporting period, the team received fewer than 100 inquiries, the majority of which were concerned about the restriping of lanes. Other major themes were: general project questions; questions about express lanes management (i.e. discount for veterans, HOV drivers, or energy efficient vehicles; equity for underserved populations; maximum and minimum price for toll); concerns about tree removal at Enfield; concerns about the status of Sound Wall #3; questions about schedule and lane closures; and numerous inquiries about whether workers were present at specific times due to the issuance of speeding tickets to drivers.

Specific comments included:

- *“On north-bound MoPac starting at Enfield, the striping on the roadway is confusing, causing cars to drive erratically, or to straddle lanes. The "new" lane demarcation is not distinguishable from the old striping. Serious safety issue, especially for inexperienced drivers.”*
- *“Please revisit the removing the original lane dashes in the construction zone. The original lane lines look to have been painted over and the new lane lines are non reflective paint with reflectors in between. In the mornings traveling North the original lines are shiny and highly visible from the sun's reflection and the new lines are hard to see causing cars to veer and swerve in traffic.”*
- *“The north bound lane on MoPac starting at the Enfield Road exit and going through at least the Windsor Road exit, the old lane stripes are visible, especially during a rain and in the dark. It is very confusing as to where your lane is, especially for those merging onto the highway. Please cover-up the old lane markings.”*
- *“Due to construction, lanes on Mopac have been shifted. The re-striping of lanes on Mopac Northbound between 183 and Parmer was done poorly when compared to the rest of the highway. Previous lines are still highly visible, making it extremely difficult to distinguish between the new lanes and old lanes, even with heightened attention. As a commuter in this area, myself and other drivers are switching lanes unintentionally on a daily basis causing dangerous situations. When the sun is shining, the situation is even worse. I ask that the old lane stripes please be removed or more effort is made to reduce their visibility.”*
- *“On June 13 at 2:09AM, approximately how many workers were actively working, and where were they working (including the cross street(s) with Mopac and whether on the North or South bound lanes)? “*

- *“I was wondering where do I send bills for lost time sitting in rush hour traffic to? With lane capacity impacted by all the construction barriers, congestion has gotten ridiculous to the point where I see a lot of motorists using shoulders to navigate MoPac mayhem during peak periods. Is there at least something you can do to speed up construction.”*

Of note, specific to the restriping issue, the team has consistently responded to the community and media that we’ve gone back and remasked the old striping, utilized traffic buttons to clarify lane channeling, and we continue to investigate industry best practices.

- The **project website** (www.MoPacExpress.com) continues to be updated every week with detailed information about work activities planned for the upcoming week and how they are expected to impact drivers.
- The team continues to monitor **social media** and initiated several discussions on Twitter and Facebook regarding construction activities. At the end of the reporting period, Twitter had 2038 followers and Facebook had 139 likes.
- The **Earthcam cameras** are live and a feed is transmitted to the project website. There is a camera at MoPac and 45th Street and another at MoPac and 10th Street, and each camera shows a shot of four different views of the construction activities occurring.
- The **smartphone application** (available for Android and iPhone) as well as the **email and text alerts** are being updated and disseminated to members of the public who sign up for these services on the website. Alerts are issued anytime there is a major lane closure that is expected to cause backups. The website is now set up to display major alerts on the front page when they occur.
- The **Highway Advisory Radio (800 AM)** is live and updated every week with a new message about the work activities planned for the upcoming week and promotes other important topics such as work zone safety. The radio broadcast is advertised on multiple signs in the corridor as well as on the project website.
- **E-Newsletters** and the **MoPac Man blog** are distributed periodically to the mailing list with a project update. Within the reporting period, an E-Newsletter launched on May 14; a Blog was posted on April 10 and June 12.
- **Media coordination** continues and fewer stories have been printed and aired this quarter compared with last quarter. KLBJ had a story the week of April 7 which highlighted a general project update. The team continues to promptly respond to any media inquiry.
- **Business outreach** continues and the team continues to identify additional outreach opportunities. Presentations were given to the Austin Board of Realtors on May 28 and to the Platinum Top 50 Real Estate Group on May 30.
- **Community outreach** continues. The team provided presentations to the Capital Area Metropolitan Planning Organization (CAMPO) on April 14, to the Austin Contractors and Engineers Association Symposium on April 16, to the Westminster Retirement Community on May 22, and to the Highland Park West Balcones Area Neighborhood Association on June 16. The team continues to schedule additional presentations to be held throughout the year.

- **Adjacent project coordination** with the Mobility Authority’s multiple ongoing environmental studies continues. “MoPac Man” attended the MoPac South Open House in April, and plans to be at the 183 North Open House in July. In addition, the project had a display table at the Mobility Authority’s Manor Expressway Grand Opening on Saturday, May 17.



**CENTRAL TEXAS
Regional Mobility Authority**

AGENDA ITEM #14 SUMMARY

Quarterly briefing on the following projects:
Maha Loop/Elroy Road, 183/183A
Intersection, SH 71 Express, Bergstrom
Expressway, Oak Hill Parkway, MoPac
South, MoPac Intersections, 183 North, and
SH 45 SW.

Strategic Plan Relevance:	Regional Mobility
Department:	Engineering
Associated Costs:	N/ A Briefing Only
Funding Source:	Toll Equity Grants, Rider 42, STP MM (CAMPO)
Board Action Required:	No

Description of Matter: Staff will provide a summary of project activities from April through June, 2014 for the following projects:

- Maha Loop/Elroy Road
- 183/183A Intersection
- SH 71 Express
- Bergstrom Expressway
- Oak Hill Parkway
- MoPac South
- MoPac Intersections
- 183 North
- SH 45 SW

Reference documentation:
GEC Quarterly Activities Reports

Contact for further information:
Wesley M. Burford, P.E., Director of Engineering
Melissa Hurst, Community Outreach Manager



CENTRAL TEXAS
Regional Mobility Authority

ENVIRONMENTAL STUDIES

**Quarterly
Progress
Report**

July 2014

BERGSTROM EXPRESSWAY

OAK HILL PARKWAY

MOPAC SOUTH ENVIRONMENTAL STUDY

MOPAC INTERSECTIONS ENVIRONMENTAL STUDY

183 NORTH MOBILITY PROJECT

SH 45SW ENVIRONMENTAL STUDY



CENTRAL TEXAS
Regional Mobility Authority

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ATKINS

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Bergstrom Expressway

INTRODUCTION

This report documents and describes the Bergstrom Expressway Project (US 183 South) for the period through April 30, 2014. This Project is currently in the environmental clearance phase of development which requires the completion of an Environmental Assessment consistent with the regulations contained in the National Environmental Policy Act of 1969 (NEPA). The Environmental Document is currently being developed in house by Texas Department of Transportation (TxDOT) Austin District staff and supported by the Central Texas Regional Mobility Authority, with the Public Outreach being led by the Mobility Authority. In addition, the Project has been authorized by legislation to use a Comprehensive Development Agreement (CDA) project delivery method. This process promotes innovative financing methods that will allow the Project to be funded and constructed much earlier than traditional methodologies.

PROJECT DESCRIPTION

The Project corridor begins just south of the Manor Expressway and extends to SH 71 East; the corridor includes seven grade separations, and connects to three major interchanges at the Manor Expressway, Airport Boulevard, and SH 71 East. The corridor includes two pedestrian bridges and a continuous shared use path connecting local communities. The Project can be considered the Gateway to Austin from the Airport and is depicted in the location map below.



The US 183 South Corridor from US 290 East to SH 71 East is one Austin's most significant arteries. Since US 183 was constructed in the mid 1960's, the roadway has been a four-lane, divided highway with minor improvements. As the primary route to and from the Austin Bergstrom International Airport and points beyond, it attracts numerous motorists per day. In recent years, this vital corridor has grown increasingly congested as the region's population has expanded. The situation is projected to get even worse in the future. Meanwhile, funding challenges at the federal and state levels have made it increasingly difficult to finance the corridor.

As part of the NEPA process, the identified needs are to reduce congestion, improve safety and utilize innovative funding and financing processes to expedite project development and construction. Along with the NEPA process, the Mobility Authority is utilizing a Public Involvement and Outreach Program that includes hosting "brown bag" lunch and one-on-one meetings to obtain agency and stakeholder buy-in along the corridor. The Environmental Assessment document is currently being prepared by the TxDOT Austin District. The Mobility Authority, with Atkins under their General Engineering Consultant contract, is supporting and coordinating closely with TxDOT as the document is being developed.

RECENT ACTIVITIES

The Project Development process is currently focusing on five major elements: Environmental Assessment (EA), Final Design Schematic, Public Outreach, Final Context Sensitive Solutions (CSS), and Design-Build Procurement.

The Mobility Authority is currently working with TxDOT and FHWA to expedite agency review and comment periods in order to obtain an Environmental Finding in Spring of 2015.

The Final CSS Open House along with the Formal Public Hearing (required by NEPA) is planned for mid-2014. The Final Design Schematic will be presented at the Public Hearing.

As part of the Project Development activities, the CTRMA team has initiated the procurement process with the Request for Qualifications issued on April 14, 2014 and an anticipated Best Value Section in Spring of 2015. In support of project financing, the CTRMA team is currently developing an update to the TIFIA Application with an estimated funding request of up to 49% of the total eligible expenses.

The Mobility Authority is currently supporting TxDOT in the development of the EA:

- Development of exhibits required by NEPA for the document
- Facilitating and coordinating meeting with other Agencies
- Developing, updating and tracking with Primavera scheduling software
- Evaluating impacts and addressing issues relevant to project development
- Supporting in recovery plans in order to meet critical dates
- Complete first Open House with Final Public Hearing in mid-2014

The Mobility Authority is advancing planning and funding activities:

- Advancing right-of-way & utilities with long lead times including coordination and funding
- Evaluating phasing schemes and financing models including potential interim milestone opening dates
- Update to TIFIA Application

The Mobility Authority has completed the Design Schematic:

- Completed the (100%) Final Design Schematic, under FHWA Review
- Completed and addressed design issues and community inquiries
- Completed Design of Value Engineering Study results
- Facilitating and coordinating meeting with other Agencies

The Mobility Authority's Stakeholder and Outreach is comprehensive and currently includes:

- Agencies: FHWA, TxDOT, City of Austin, Travis County
- Businesses: Misc. Land Developers / Property Owners, Freescale , YMCA, AT&T
- Home Owners Associations: Knollwood , Senate Hills , University Hills
- Interest Groups: Gateway Oaks, Heritage Tree Foundation, PODER, El Concilio
- Completed 1st and 2nd Round of Outreach Activities
- Performing required follow-ups with Businesses, Home Owners and Interest Groups

The Mobility Authority's Context Sensitive Solution process is currently focusing on:

- Integrating the facility with the area's built environment to enhance community and in particular the area's economical quality of life
- Integrating the natural environment in order to maximize the facilities scenic, recreation and aesthetic qualities
- Incorporating aesthetics that fit the facilities physical setting and preserve the scenic, aesthetic, neighborhood, historic and environmental attributes of the area
- Including innovative design elements including considerations from the Green Mobility Challenge
- Completed 2 CSS Open Houses, Final Results to be presented at Final Open House in mid 2014

The Mobility Authority's Design Build Procurement process is currently scheduled as:

- Request for Qualifications issued April 14, 2014
- Board Approval Shortlisted Proposers, Fall 2014
- Issuance of Final Request for Detailed Proposals, Winter 2014
- Board Approval of Best Value Proposer, Spring 2015

SCHEDULE

The Environmental Assessment process was launched in late 2011 and is anticipated to be complete in spring 2015. The Procurement was launched this month and should be complete in spring of 2015. A detailed schedule discussion, including the tracking of intermediate milestones, will be provided in upcoming Quarterly Reports. Below is a summary of the anticipated timeline that is being communicated to the public.





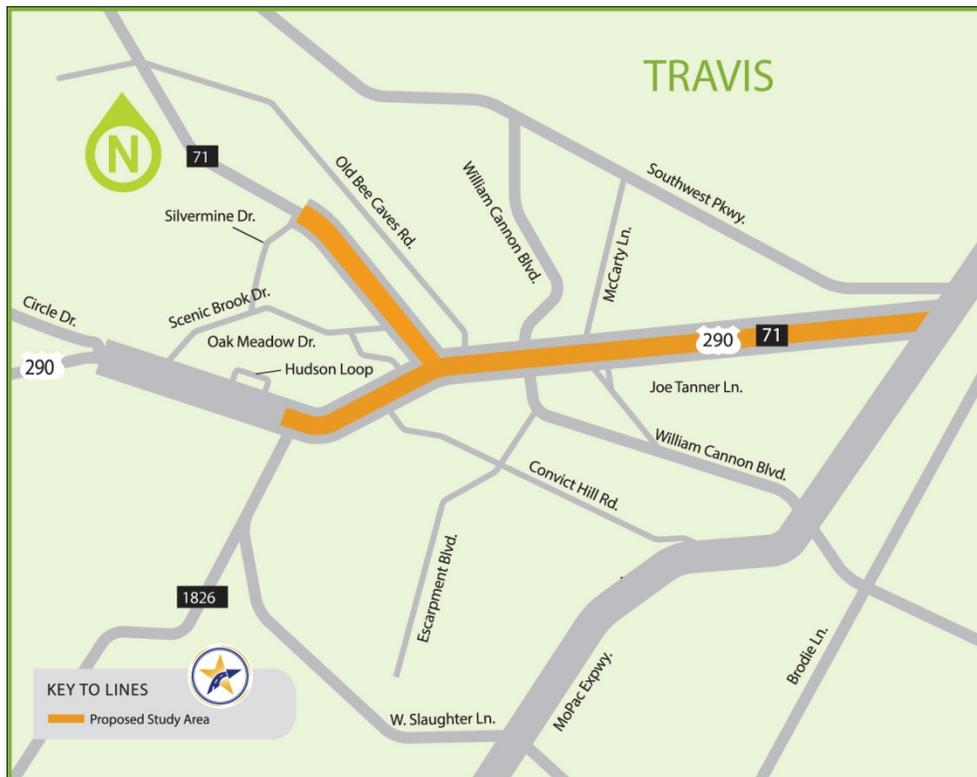
Oak Hill Parkway

INTRODUCTION

This report documents and describes the Oak Hill Parkway Project for the period through June 30, 2014. This Project is currently in the environmental clearance phase of development which requires the completion of an Environmental Impact Statement (EIS) consistent with the regulations contained in the National Environmental Policy Act of 1969 (NEPA). The Project is currently being developed jointly by the Central Texas Regional Mobility Authority and the Texas Department of Transportation (TxDOT) Austin District.

PROJECT DESCRIPTION

The Project encompasses the area surrounding the existing “Y at Oak Hill” intersection and includes the study of US 290 West from FM 1826 to Loop 1, including SH 71 West from Silvermine Drive to US 290 West. The study area is depicted in the location map below. A transition may be necessary through Circle Drive to ensure adequate and safe operations of the facility.



Currently this area experiences large-scale congestion while attracting more than 60,000 cars/trucks a day and serving as a corridor for statewide travel. As part of the NEPA process, nine reasonable alternatives along with two options that could be applicable to multiple scenarios have been identified thus far to address the issues currently plaguing the study area. These alternatives, along with a “No Build” scenario, will be screened to assess the direct, indirect, and cumulative environmental impacts

such that environmental values are considered alongside the technical and economic factors. Another important element of the NEPA process, which is also consistent with the Mobility Authority's approach to project implementation, is public involvement; throughout the entire Environmental Impact Statement process, input will be solicited from neighbors and drivers of the corridor that could potentially be affected.

The Environmental Impact Statement document is currently being prepared by a consultant team led by Rodriguez Transportation Group, Inc. (RTG) under a professional services agreement with the TxDOT Austin District. The Mobility Authority, supported by Atkins under their General Engineering Consultant contract, is coordinating closely with both TxDOT and RTG as the document is being developed.

PREVIOUS EFFORTS

As part of the Public Involvement Initiation, several critical elements were advanced in the previous quarter:

- On January 27th, the team began outlining the framework for the Context Sensitive Solutions (CSS) process and the schedule for implementation.
- On February 4th, a public workshop was held to discuss extending the transition on the western end and to seek input from stakeholders about their needs and concerns regarding that transition.
- On March 22nd, a public funding workshop was held to educate stakeholders about funding sources available for added capacity projects.
- Monthly e-newsletters have been distributed to announce the opportunities for public input and to keep the stakeholders informed of activities and progress.

RECENT ACTIVITIES

The implementation of the Project is currently focusing on actively engaging the public to help us identify the project constraints and initiation of early concept development through the use of citizen workgroups. This quarter's activities included:

- The comments on the western transition from the February 4th workshop were taken into consideration, and a design was developed that would provide an innovative intersection design at Circle Drive minimizing the right of way footprint needed. A follow up workshop was held May 27th to present the new design to stakeholders. Refinements were made as a result of comments at the workshop, and the refined design will be carried forward on all concepts.
- Based on comments received at the May workshop, separate follow-up meetings were held with various property owners and public entities that expressed a desire for additional information on the entire project.
- The screening criteria were finalized on April 14 in collaboration with FHWA and ENV.
- Analysis was completed on each concept to be able to perform the screening, and the implementation of the criteria resulted in all concepts dropping out but two: Concepts A & C.
- An Open House was held June 17th at Covington Middle School to discuss the results of the screening process and seek comments regarding such.
- The comments from the Open House are being reviewed to determine if additional actions are needed.
- Ongoing analysis continues regarding the potential for a regional detention system

- More detailed information and notes from the public involvement activities can also be found on the project website www.oakhillparkway.com.

PROGRESS STATUS

The Environmental Impact Statement process is still in the first year of development and is anticipated to take approximately four years to complete. To date, the project team has performed consistent with the project schedule.



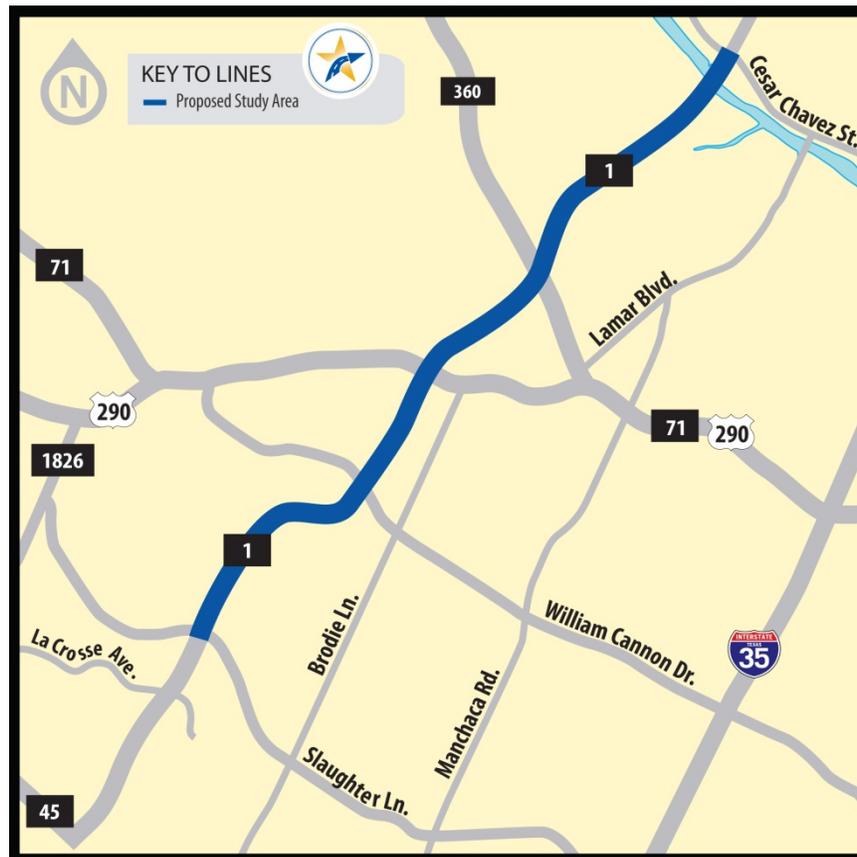


MoPac South Environmental Study

MoPac South
ENVIRONMENTAL STUDY

PROJECT DESCRIPTION

MoPac Expressway south of Cesar Chavez Street is a vital transportation artery for Austin. Initial construction started in 1973 and various sections have been under construction for the past 40 years. Currently this eight mile section is a four to six lane freeway which attracts up to 150,000 cars and trucks per day on the north end. Over time, growth in the corridor – residential, retail and commercial – has led to increased congestion and reduced mobility. Funding for an environmental study along with preliminary and final design was allocated to the project under Rider 42 of the General Appropriations Act (2011, HB 1, 82nd Legislature, Regular to the Session). Rider 42 funds are state funds from Proposition 12 set aside to study the most congested roadways in Texas.



The Mobility Authority is leading the environmental study and community outreach program which launched in April 2014 in partnership with the Texas Department of Transportation (TxDOT).

The environmental study will thoroughly analyze the corridor from Cesar Chavez Street to Slaughter Lane and determine the best alternatives for improving mobility. The result will be documented in an Environmental Assessment that will consider “build” and “no build” options, as well as traffic management strategies. Over the course of the study, a full range of alternatives will be developed that will take into account the needs of drivers, transit riders, bicyclists and pedestrians as well as surrounding businesses, neighborhoods and the environment. Through extensive analysis of the engineering feasibility, social, economic and environmental impacts of each alternative, as well as consideration of public input, the study team will recommend a solution.

This report describes the status of the MoPac South Environmental Study and documents the activities accomplished through the second quarter of 2014.

RECENT ACTIVITIES

The following activities have been accomplished by the Mobility Authority and its consultants during the reporting period.

PROJECT DEVELOPMENT

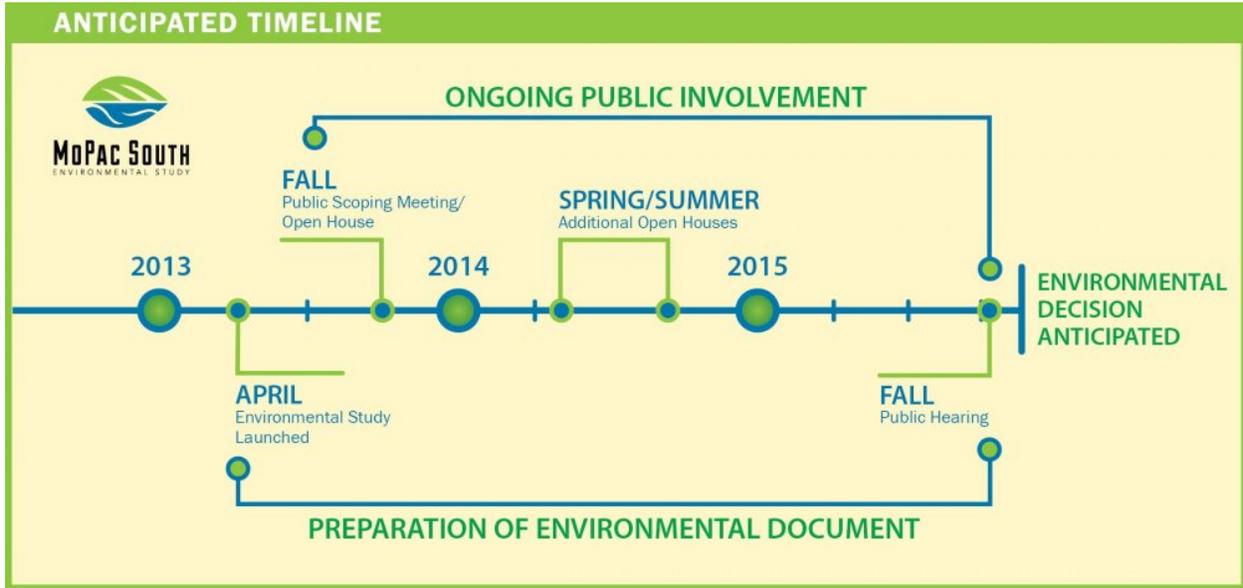
- The first Technical Working Group meeting was held April 16 in order to preview the second Open House information, including the latest draft of the: Purpose and Need, preliminary alternatives, and the evaluation criteria for both preliminary and reasonable alternatives.
- Initial traffic data has been developed and evaluation of Preliminary Alternatives against evaluation criteria will begin.
- Technical memoranda are being prepared for the alternative analysis as well as for the social, economic, and environmental impact evaluations.
- Engineers are initiating sketch level designs.
- Initial draft environmental assessment submittal is planned for early 2015 with a public hearing planned for the Summer of 2015.

COMMUNITY OUTREACH

- The second Open House was held on April 29 at Barton Creek Square Mall. Over 120 members of the public attended. Bob Moore representing Travis County Commissioner Gerald Daugherty and Jake Cottingham for State Representative Donna Howard were also in attendance. The public was asked to provide input on the refined Purpose and Need, preliminary alternatives, and evaluation factors to screen alternatives.
- A concurrent Virtual Open House launched on April 29 on the project website for those who wished to download and review the Open House materials online and provide their comments electronically. During the comment period, the site received 414 unique page views that resulted in 56 online surveys being submitted. The official comment period for Open House No. 2 closed May 9.
- The Context Sensitive Solutions (CSS) process will be implemented over the next year.
- The next public meeting is planned for early 2015.

SCHEDULE

The environmental process is anticipated to take two to three years.



MoPac Intersections Environmental Study

PROJECT DESCRIPTION

The Mobility Authority, in coordination with the Texas Department of Transportation (TxDOT), will prepare an environmental study document for proposed operational and safety improvements including grade separation of the MoPac mainlanes under Slaughter Lane and La Crosse Avenue. The environmental study launched in May of 2013. If the proposed improvements are environmentally cleared, the project will be turned over to TxDOT to design, construct, and maintain.



This report describes the status of the MoPac Intersections Environmental Study and documents the activities accomplished through the second quarter of 2014.

RECENT ACTIVITIES

The following activities have been accomplished by the Mobility Authority and its consultants during the reporting period.

PROJECT DEVELOPMENT

- The schematic design and the majority of the technical memoranda have been submitted to TxDOT for review and comment.

COMMUNITY OUTREACH

- The team is currently preparing for a community event during Fall/Winter 2014.

SCHEDULE

An environmental decision is anticipated in Winter 2014/2015.

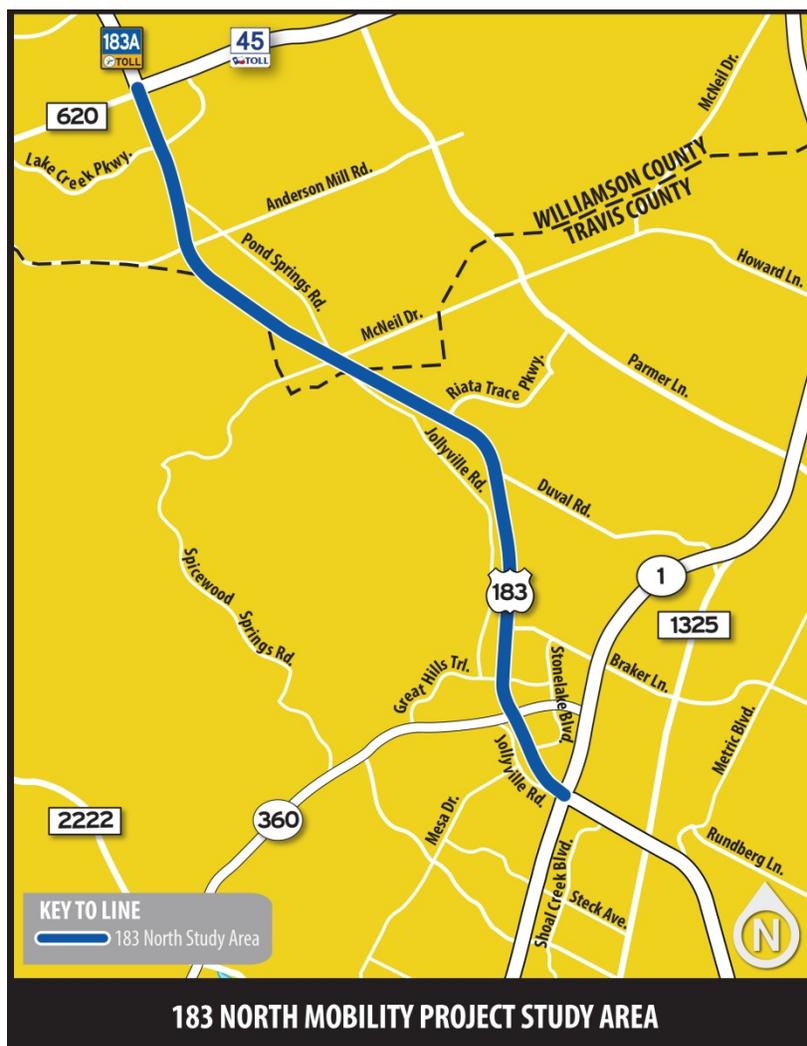
183 North Mobility Project



PROJECT DESCRIPTION

In August 2013, the Central Texas Regional Mobility Authority (Mobility Authority) and the Texas Department of Transportation (TxDOT) launched the 183 North Mobility Project. This new environmental study will thoroughly analyze an eight-mile segment of US 183 between SH 45 North and MoPac and determine the best options for managing congestion, as well as improving transit reliability and emergency response times. The study will also look at ways to connect mobility improvements on 183 North to the MoPac corridor.

The 183 North Mobility Project was selected as one of the recipients of the CAMPO Surface Transportation Program – Metropolitan Mobility (STP-MM) funding program. Proposed improvements could include adding lane(s) along existing US 183 North (from SH 45 North to MoPac), as well as direct connectors from US 183 and MoPac.



Proposed improvements could include adding lane(s) along existing US 183, as well as direct connectors from US 183 and MoPac. This summer, the team announced that the alternatives being carried forward for further study are Express Lanes and the No Build alternative. A decision on what solution will be implemented is expected in early 2016.

This report describes the status of the 183 North Mobility Project and documents the activities accomplished through the second quarter of 2014.

RECENT ACTIVITIES

The following activities have been accomplished by the Mobility Authority and its consultants during the reporting period.

PROJECT DEVELOPMENT / PROCUREMENT

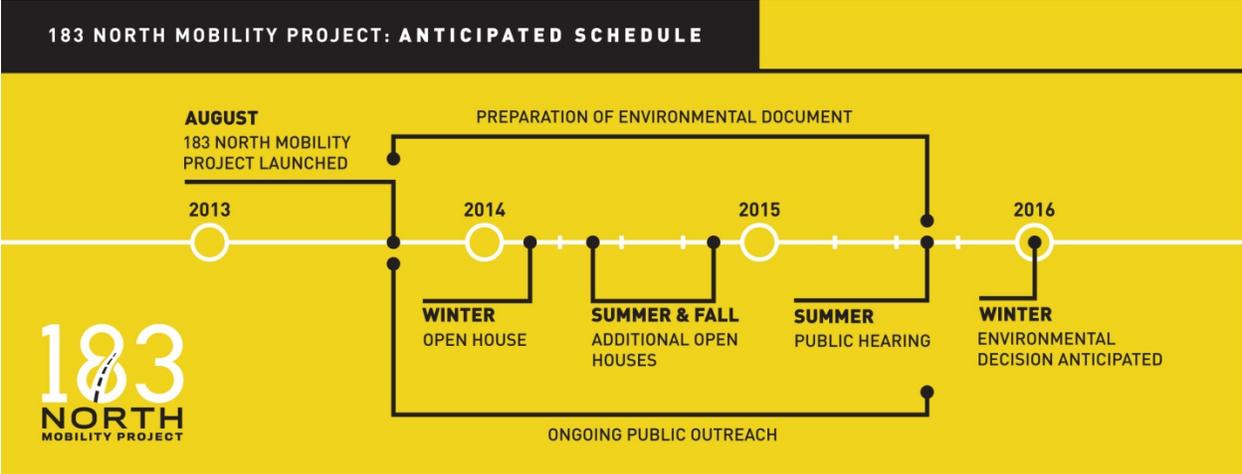
- The team continues preparation of environmental documentation, primarily data collection and public involvement activities, as well as schematic efforts.
- Comments from TxDOT and FHWA on the draft Purpose and Need and have been addressed and the updated draft is being circulated as of June 9.
- A second Technical Working Group was held on June 24 in advance of the July Open House.
- Project preliminary design development and traffic operational analysis is on-going. A Design Concept Conference meeting has been scheduled with TxDOT for June 9 and the draft Design Exception package is being developed.
- Initial traffic modeling micro simulations for the US183/MoPac interchange have been completed and reviewed; refinements are currently under way. Preliminary results of Origin and Destination Study have been submitted, reviewed and comments provided.

COMMUNITY OUTREACH

- Consideration of input from the stakeholder meetings and the Open House is underway in the refinement of the Purpose and Need and alternatives.
- Planning continues for an Open House No. 2 to be held July 8 at Westwood High School. The following will be available for public input: refined Purpose and Need, reasonable alternatives (build and no build) and evaluation factors to screen alternatives.
- The team is planning a “ride along” with City of Austin and city bicycle/pedestrian groups for July 7 to identify possible locations for bike and pedestrian improvements.

SCHEDULE

The environmental process is anticipated to take two to three years. The schedule for this project anticipates a public hearing to occur in August 2015, and a completion of the environmental study in early 2016.



SH 45 SW

ENVIRONMENTAL STUDY



SH 45SW Environmental Study

PROJECT DESCRIPTION

The Texas Department of Transportation (TxDOT) is working with the Central Texas Regional Mobility Authority (Mobility Authority), Hays and Travis counties, and regulatory agencies to study a possible solution for improving mobility in the rapidly growing area of northern Hays and southern Travis counties. This combined effort includes an environmental study of an area between MoPac and FM 1626. The study will consider environmental impacts, constraints, the needs of drivers, and concerns of surrounding neighbors. The study, which will result in an Environmental Impact Statement, launched in June 2013.

TxDOT's Special Projects Division and Environmental Affairs Division are leading the project; with the Mobility Authority in a support role. Community outreach is being led by the Mobility Authority.

This report describes the status of the SH 45SW Environmental Study and documents the activities accomplished through the second quarter of 2014.

RECENT ACTIVITIES

The following activities have been accomplished by the Mobility Authority and its consultants during the reporting period.

COMMUNITY OUTREACH

- The Mobility Authority continues to manage the project website (www.SH45SW.com) and twitter account (@SH45SW) as well as answer the phone hotline (512-593-4202).
- TxDOT provided a presentation on the project at the Kent Butler Summit on April 25
- The Draft Environmental Impact Statement was released for public review on June 27.
- The public hearing is scheduled for July 29, 2014.

DESIGN PROCUREMENT

- The Mobility Authority posted a Request for Qualifications (RFQ) for Design Procurement on May 23. A pre-proposal conference was held on June 3. Proposals are due July 30.
- Final Design Procurement shortlist and interviews are planned for late August.

SCHEDULE

The environmental process is anticipated to take two years.





**CENTRAL TEXAS
Regional Mobility Authority**

AGENDA ITEM # 15 SUMMARY

Presentation on traffic modeling for the SH 45 SW project.

Strategic Plan Relevance: Regional Mobility

Department: Executive

Associated Costs: N/A

Funding Source: N/A

Board Action Required: No

Description of Matter:

Presentation on SH 45SW Traffic Modeling by Will Smithson, HNTB Corporation.

Reference documentation:

None.

Contact for further information:

Mike Heiligenstein, Executive Director



**CENTRAL TEXAS
Regional Mobility Authority**

AGENDA ITEM #16 SUMMARY

Executive Director's report.

Strategic Plan Relevance: Regional Mobility

Department: Executive

Associated Costs: N/A

Funding Source: N/A

Board Action Required: No

Description of Matter:

Executive Director's Monthly report

- A. Interlocal agreement with the City of Leander and development project adjacent to the 183/183A Intersection.
- B. City of Austin City Council candidate presentation.

Reference documentation:

Executive Director's report

Contact for further information:

Mike Heiligenstein, Executive Director



CENTRAL TEXAS
Regional Mobility Authority

REPORT TO THE BOARD OF DIRECTORS

JULY 30, 2014

Mike Heiligenstein – Executive Director

Maha Loop Groundbreaking

On July 9, the Mobility Authority joined with Travis County Precinct 4 Commissioner Margaret Gomez to hold a groundbreaking of the \$16.6 million non-tolled Maha Loop extension project in East Travis County.

Executive Director Mike Heiligenstein served as emcee for the event. Board Secretary Nikelle Meade represented the Mobility Authority and in her remarks to the more than 50 guests and media present emphasized the Mobility Authority's commitment to bringing greater mobility to the region by offering solutions — both tolled and non-tolled — that help people get where they want to go.



July 9 Maha Loop Groundbreaking

The almost two-mile stretch of road will connect Pearce Lane to State Highway 71. It will also be an important artery to reduce congestion around the Formula One track.

The Mobility Authority was approached because of our reputation for delivering projects on budget and on time, and because we have the design-build authority to construct projects like this.

This project represents what the Mobility Authority is all about — it's not about tolling, it's about finding mobility solutions. Constructing this project is part of the Mobility Authority's mission to "Implement innovative, multi-modal transportation solutions that reduce congestion and create transportation choices that enhance quality of life and economic vitality."

This project is important for East Travis County, and will enhance the quality of life and economic opportunities for the area. Thanks to the Board's leadership on this project, we have once again proven to the community our ability to get things done. The road is expected to be open by next spring.

Industry News:

Tolled Express Lanes a National Trend, and Colorado's Firmly on Board

Excerpted article from KUNC radio, Greely, CO



An express toll lane in Utah on southbound I-15. Note the electronic sign that notifies drivers of the variable price of the toll.

On a June day in 2006, Myron Swisher stood on a highway overpass, watching a history-making moment on Interstate 25. Swisher, who worked for the state Department of Transportation, had labored for the past nine years to open a high occupancy toll lane on the crowded road, and he wanted to watch the first cars use it.

"It was probably about 2:30 in the afternoon, so I wanted to get out there and see how things are going before rush hour started," Swisher said.

Looking back, Swisher's moment on the bridge may have marked the beginning of a new era in Colorado transportation.

The state is now in the midst of planning or constructing at least four new express toll lanes on Front Range highways. It's not alone: transportation experts say the trend toward tolling new highway lanes, begun in California, is spreading to congested cities across the country.

"We're seeing an explosion of these projects across the country to help create mobility in these congested corridors," said Chuck Fuhs, a Houston-based consultant with 40 years of experience in the transportation industry.

The reason for this is twofold, said Fuhs and others who watch trends in transportation. For years, transportation departments would build new lanes, only to see them fill up almost immediately. Tolloed lanes, often with prices that change according to demand, are a better way to ensure drivers have a reliable trip. The other big reason for expanding toll lanes has to do with widespread public unwillingness to raise the gas taxes that pay for transportation projects. For agencies lacking money to build a new lane from scratch, tolloed lanes are a way to partially pay for new road efforts.

Dynamic Pricing Affects Driver Behavior

Tolls also serve an important purpose: managing demand by price. The basic idea: In order to keep traffic flowing in the express lane, there should only be a certain quantity of cars. If there is a lot of extra room in the lane, the price to enter is low. If space is getting tight, the price increases, discouraging drivers from entering.

"It's similar to how you would pay more for an airplane ticket during holidays," said Gian-Claudia Sciara, of the Urban Land Use and Transportation Policy Center at the University of California, Davis.

In the early days of the movement, transportation departments often took underused existing High Occupancy Vehicle lanes, and transformed them to toll lanes. The lanes Swisher worked on for I-25 were former HOV lanes. Now, more and more cities are adding lanes knowing at the outset they'll be tolloed.

Disapproval at First, But Opinion Often Shifts

In any city, the announcement that express toll lanes are coming is typically met with opposition, said Ginger

Goodin, a senior research engineer at Texas A&M's Transportation Research Institute. Goodin has evaluated express toll lane projects in many cities, including Seattle, Los Angeles and Houston. Goodin knows of over 20 express toll lanes in existence in at least 12 cities across the country, and said 20 more are in development.

Often during toll lane construction, she said, "you start seeing dropping public support. But what we have also mapped, once it is in place and people start using it, six months to a year, a ramp-up period, then you get public support."

While some toll lanes have bumpy starts and need tweaking, both Goodin and Fuhs, the consultant, said most of them end up working well and often gaining in popularity over time.

"The difficult part is, can you ride that wave of negative criticism. There's going to be a lot of pressure to just open the lane to everybody," said Goodin.

One of the most common criticisms of such lanes is that they are "Lexus lanes," giving the wealthy a faster transit time while the 99 percent suffers in the slow lanes. But studies have found that drivers from a wide range of incomes end up using the lanes when they really need a reliable travel option. Also, by offering a faster option for buses, those that use public transit also often benefit.

Events and Activities

- Mike Heiligenstein spoke to Austin City Council candidates on July 8 to provide an overview of Mobility Authority plans and projects.
- Work will begin on the week of July 28 on the 183A frontage road asphalt maintenance project. The project entails removing and replacing the very top 2" of an overall pavement section that is about 18" thick. We anticipate the process to be finished by the end of October 2014. Some work will occur at night to minimize disruption to the traveling public and ongoing traffic advisories will be posted online and sent via email.
- Mike Heiligenstein and the project team met with Leander developers about significant residential and potential commercial development projects along the 183A/US 183 intersection improvement project. (See details in enclosed attachments.)
- July 29, SH45SW Public Hearing, Bowie High School, 5 p.m.
- Sept. 24, Board of Directors meeting, 9 a.m.



PROJECT UPDATES

MoPac Improvement Project Construction

- The MoPac Improvement Project is on schedule.
- Final design and the permitting process scheduled to be completed by end of July.
- Work on Segments 1 & 2A (Parmer Lane to 183) included subgrade grading and drilling shafts for Capital Metro bridge widening.



Retaining Wall Construction in Median at RM 2222

- Work on Segment 2B (183 to RM 2222) included: installation of electrical and ITS conduit, storm drains, and temporary signs; construction of retaining walls; and substructure work for RM 2222 bridge widening.
- Work on Segment 3 (RM 2222 to Enfield) included: construction of temporary ramps; utility relocations in advance of sound wall construction; and construction of columns and abutments for 45th, and Windsor Road bridges; completed substructure for widening of southbound Enfield bridge.
- Work on Segment 4 (Enfield Road to Ladybird Lake) included: subgrade earthwork; installation of storm drain facilities; construction of Enfield Road temporary ramp; roadway widening; and construction of bore pits for utility relocation.
- Pre-casting of beams and sound walls continues. Inspection of operations at the casting plant was conducted members of the oversight team.
- Mowing operations continue.
- CH2M HILL has been providing a courtesy vehicle (similar to HERO) during peak hours and during construction as well as handling the maintenance of the corridor (graffiti removal, garbage pick-up,

guardrail repair). They will continue this until final acceptance of the project.

- MoPac Man continues to update the website daily with closure information and has responded to numerous e-mails and tweets. His 800 AM broadcasts are updated weekly with closure information as well as information about the upcoming express lanes.

MoPac South Environmental Study

- Technical memoranda are being prepared for the alternative analysis as well as for the social, economic, and environmental impact evaluations.
- Stakeholder meetings continue, including a meeting with the City of Austin to discuss Bike and Pedestrian issues .
- Engineers are Initiating sketch level designs.
- The next public meeting is planned for early 2015.
- Initial draft EA submittal is planned for early 2015 with a Public Hearing planned for the Summer of 2015.

MoPac Intersections Environmental Study

- The Schematic design and the majority of the technical memoranda have been submitted to TxDOT for review and comment.
- A public event is planned for Fall/Winter 2014.

183 North Mobility Project

- Purpose and Need Statement was submitted to TxDOT and FHWA for review and comment on June 9.
- A second Technical Working Group was held on June 24.
- A ride along with City of Austin and city Bike/Ped groups was held on July 7 to identify possible locations for bike and pedestrian improvements.
- Approximately 100 members of the public attended the July 8 Open House. The Virtual Open House was available on the project website between July 9 -18.
- Project preliminary design development and traffic operational analysis is on-going.
- Project on schedule for Public Hearing Aug 2015.

SH 45SW Environmental Study

- Public hearing is scheduled for July 29.
- Final Design Procurement shortlist and interviews are planned for late August. Construction is anticipated to start in Late 2015/Early 2016.

Bergstrom Expressway Project

- The Draft Environmental Assessment (EA) is currently under review by Federal Highway Administration, with a Public Hearing scheduled for mid-2014 and an EA finding in early 2015.
- Final Design Schematic is currently under review by Federal Highway Administration with anticipated approval in September 2014.
- Public involvement activities continue as the team prepares for the Final CSS Open House scheduled in August 2014.
- We have initiated the procurement process with the Request for Qualifications issued in April, anticipated Shortlist of Proposers in September and Best Value Section in early 2015.
- Project programming activities continue as the team works to finalize Project Funding Agreements with TxDOT and FHWA.
- Other Project programming efforts have begun in order to meet Federal requirements such as Project Management Plan, Initial Finance Plan, and the Cost Estimate Review Workshop.
- Financing activities continue as we develop the updates to TIFIA Application and begin the Investment Grade Traffic & Revenue Study.
- Outreach and status reporting activities continue as the team meets with the stakeholders including Public Officials, Agencies and Community Organizations.

Maha Loop/Elroy Road Phase One



Maha Loop July 8 Groundbreaking Ceremony

- A Notice to Proceed for construction was issued June 23.
- Contractor began on July 8 to drill shafts on south bridge.
- Change order was made to adjust fence elements to comply with property owner agreement negotiated by Travis County after bid award was approved by Travis County (\$73k).

Manor Expressway

- CTMC is currently addressing Punch List items in order to obtain final acceptance and finalizing the non-conflict utility relocations required by the project.

Oak Hill Parkway



OAK HILL PARKWAY

- TxDOT, TTI and the Mobility Authority are working together to memorialize some of the public funding workshop information through an information song that would be generic enough for use on other projects.
- Follow up meetings with various stakeholders have resulted in refinements to the western transition to better facilitate access for emergency services and businesses.
- An Open House was held June 17 at Covington Middle School along with a corresponding Virtual Open House on the OakHillParkway.com website from June 18 - 27. The refined and narrowed mobility concepts (A, C and No Build) were presented as those moving forward for further evaluation. Approximately 140 members from the public attended the Open House.
- The comments from the Open House are being reviewed and responses developed. Some of the comments will result in further refinements to the concepts being carried forward.
- Efforts with TxDOT and the City of Austin continue for potential detention facilities.



CENTRAL TEXAS
Regional Mobility Authority

Transportation Overview

July 8, 2014

Mike Heiligenstein, Executive Director

History of the Mobility Authority



- Created in 2002 by the Texas Legislature to improve the transportation system in Williamson and Travis counties
- Mission:
 - Implement innovative, multi-modal transportation solutions that reduce congestion and create transportation choices that enhance quality of life and economic vitality
- 7-member board of directors:
 - Chairman appointed by the Governor
 - Travis and Williamson County Commissioners courts each appoint three members
- Authorized under state law to implement a wide range of transportation systems, issue revenue bonds to fund projects, and utilize user fees and/or taxes to fund operations and repay bonds. From Rail to Trail.



We build toll roads, and so much more.

Why Toll Roads?



- **Federal and state highway funds are drying up.**
- **Tolling allows projects to be expedited.**
- **Only those who use toll roads pay for them.**
- **Free alternatives are always available – and in many cases expanded.**
- **The Mobility Authority never tolls an existing non-tolled road; we just add new capacity.**

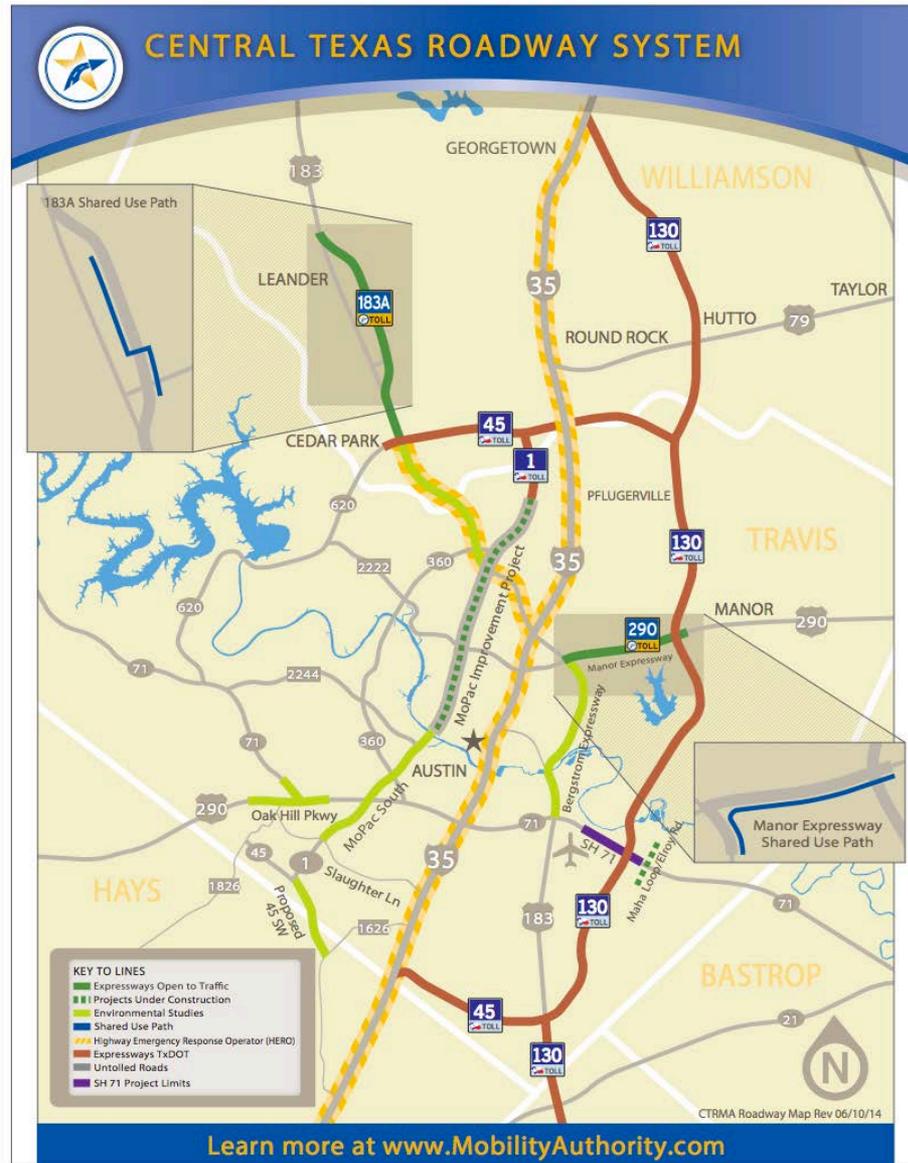


Mobility Authority Project Development 10-Year Report Card



	Tolled	Non-Tolled	Bike/Pedestrian
Completed Roads (Lane Miles)			
183A	69.6	20	11 miles
Manor/290E	38	37	6 miles
Under Construction			
MoPAC (11 miles)	22 miles*	Intersections/ Aesthetic Improvements	Estimated \$4 million in improvements
71 East	16	--	--
Under Development			
Oakhill Parkway "Y"	21	21	TBD
183 South	36	36	7
183 North	30*	All Current Remain non tolled	TBD
MoPAC South	16-30*	All Current Remain Non-tolled	TBD

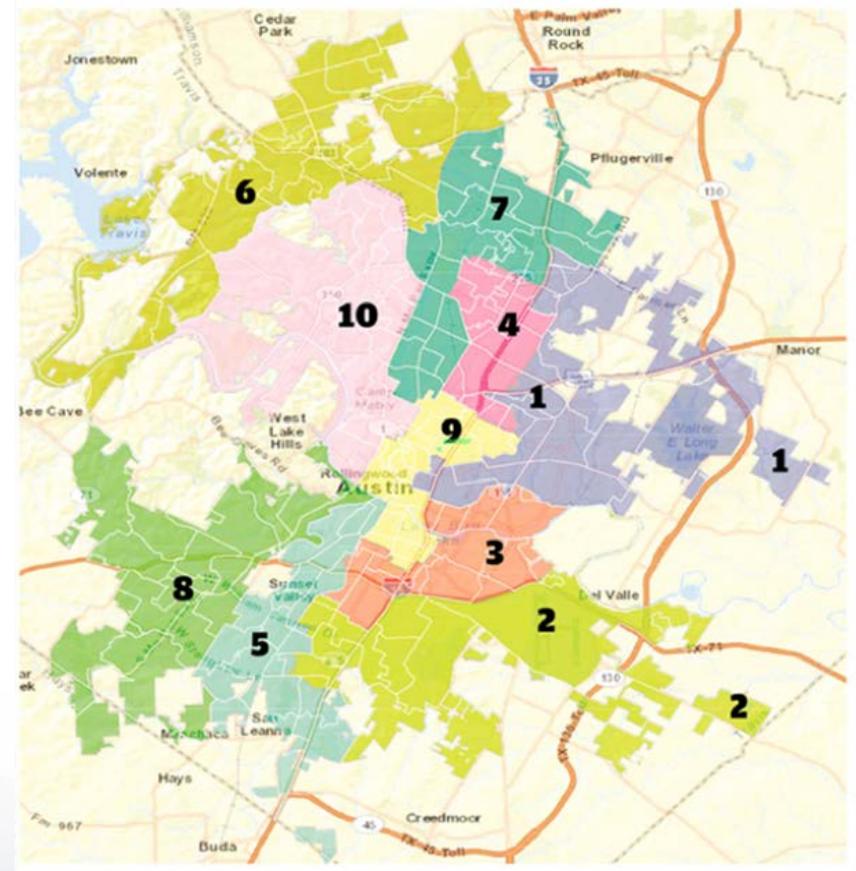
Building an Expressway Network



Our Projects & Your Districts



- **183 A – District 6 , 10**
- **Manor Expressway – District 1**
- **MoPac Improvement Project – District 7, 9, 10**
- **MoPac South – District 5, 7, 8, 9**
- **Oak Hill Parkway – District 8**
- **Bergstrom Expressway – District 1, 2 , 3**
- **183 North – District 6 , 7, 10**
- **SH 45 SW – District 5 , 8**
- **Maha Loop/Elroy Road – District 2**
- **SH 71 – District 2**



Lessons Learned, Opportunities and Challenges



- **Initially a response to citizen/community demands for better transportation planning and development. Regional leaders criticized for failure to act – failure to get “fair share” in state allocation process. Always a difficult process.**
- **Drive to join two very different counties into one Authority. Looking for alternatives to property tax to fund.**
- **What RMA offered:**
 - Local control/management (Board)
 - Revenue stays home
 - Outreach by local community leaders
 - Focus on building consent
 - Ability to build agency around entrepreneurial model (19 employees)
 - Design/Build / P3 / TRZ
 - Access to federal and state funds and loans

Facilities in operation	\$750 million
Facilities under construction	\$325 million
Under active development	\$ 2 billion

The Mobility Authority is:

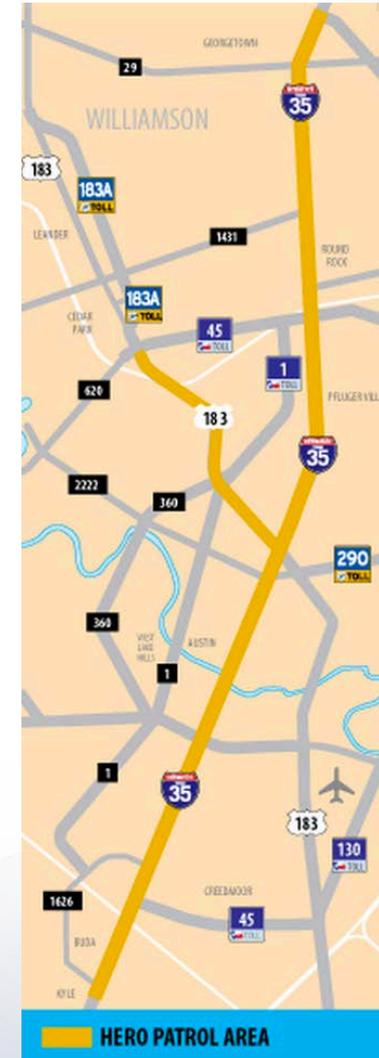


- **Building a system**
- **Leveraging scarce dollars**
- **Reaching out to the community**
- **Using technology**
- **Providing congestion relief**
- **Providing reliable alternatives**
- **Creating partnerships**
- **Building now – planning for the future**



• HERO Program

- Emergency road service to motorists along I-35 and US 183
- Monday – Friday, 6:00 a.m. – 8:00 p.m.
- Helps drivers, clears the roadway for thousands of motorists
- Allows safety and emergency vehicles to reach accident scenes quickly



More than just Toll Roads



• Shared-Use Paths

- 183A – 11 miles of SUP
- MoPac Improvement Project – 3 miles of paths
- Manor Expressway – 6 miles of paths

• Green Mobility Challenge

- Design competition for landscape architects, planners and engineers to design environmentally responsible transportation facilities

• Mobile Apps

- MoPac Improvement Project
- Carma Carpooling
- Metropia Mobile





CENTRAL TEXAS
Regional Mobility Authority



**CENTRAL TEXAS
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AGENDA ITEM #17 SUMMARY

EXECUTIVE SESSION

Executive Session:

Discuss acquisition of one or more parcels or interests in real property needed for the Manor Expressway as authorized by §551.072 (Deliberation Regarding Real Property), and related legal issues as authorized by §551.071.



**CENTRAL TEXAS
Regional Mobility Authority**

AGENDA ITEM #18 SUMMARY

EXECUTIVE SESSION

Executive Session:

Discuss legal issues relating to procurement and financing of Mobility Authority transportation projects, as authorized by §551.071 (Consultation With Attorney).



**CENTRAL TEXAS
Regional Mobility Authority**

AGENDA ITEM #19 SUMMARY

EXECUTIVE SESSION

Executive Session:

Discuss legal issues related to claims by or against the Mobility Authority, pending or contemplated litigation and any related settlement offers; or other matters as authorized by §551.071 (Consultation with Attorney; Closed Meeting).



CENTRAL TEXAS
Regional Mobility Authority

AGENDA ITEM #20 SUMMARY

***SETTLEMENT AGREEMENT FOR CLAIMS
RELATING TO ACQUISITION OF RIGHT
OF WAY***

Authorize negotiation and execution of an agreement with William K. Reagan and Reagan National Advertising of Austin, Inc. to resolve outstanding claims relating to acquisition of property interests in Parcel 17, a 0.899 acre parcel of real estate located at 9207 US Hwy 290E in Travis County, necessary for construction of the Manor Expressway (290 Toll) project.