

**January 26, 2011 CTRMA BOARD OF DIRECTORS MEETING
Summary Sheet**

AGENDA ITEM # 8

183A Phase II Project Report General Briefing

Department: Engineering

Associated Costs: None

Funding Source: None

Board Action Required: No

Description of Matter: The report is an account of the construction activities on the 183A Phase II Project construction from October through December, 2010.

Attached documentation for reference:

GEC Quarterly Activities Report and Board Presentation

Contact for further information:

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183A TURNPIKE - PHASE II
**QUARTERLY
CONSTRUCTION
PROGRESS REPORT**



No. 3 | January 2011



INDEPENDENT ENGINEERING REPORT



CENTRAL TEXAS
Regional Mobility Authority

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183A TURNPIKE - PHASE II
**Quarterly Construction
Progress Report**
No. 3



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183A TURNPIKE - PHASE II
**Quarterly Construction
Progress Report**
No. 3



INTRODUCTION

This report documents and describes the second phase of the 183A Turnpike Project construction from October 1, 2010 to December 31, 2010. This project is an extension of the existing 183A toll road facility and is being constructed by the Central Texas Regional Mobility Authority (Mobility Authority). The project is funded entirely from toll revenue bonds.

PROJECT DESCRIPTION

Phase II of the 183A Turnpike Project is located in southwestern Williamson County and extends approximately 5.1 miles, traversing through the cities of Cedar Park and Leander in the State of Texas. The Project extends the mainlanes of the existing 183A Turnpike from FM 1431 to north of RM 2243. This limited-access toll road will be constructed between the existing frontage roads – which were constructed as part of the initial phase of the Project – and the added capacity will consist of three lanes in each direction with access ramps connecting to the frontage roads. It is located east of, and parallel to, the existing US 183 facility. See Figure 1 for the Project Map.

The construction tasks principally include: preparation of right-of-way; excavation and embankment; flexible base / cement treated base; warm mix asphalt; concrete pavement; concrete curb and gutter; roadway bridges; retaining walls; drill shafts; rip rap; concrete box culverts and other drainage structures; water quality ponds; barricades, signs, and traffic handling; illumination; overhead sign supports; traffic / pedestrian signal head, pole, and detectors; a pedestrian bridge; toll facilities; and ITS ducts.

The Mobility Authority entered into a contract with Webber LLC Contractors (Webber) to construct the 183A Phase II Project. The agreement requires the project to be substantially complete by March 2012. The Contractor has developed an acceptable Baseline CPM (Critical Path Method) Schedule for the Project. The Mobility Authority issued Notices to Proceed (NTP) for NTP 1 and NTP 2 on March 24, 2010, in accordance with the terms of the contract. An Alternative Bid NTP was granted on May 7, 2010; and NTP 3 was issued on November 24, 2010.

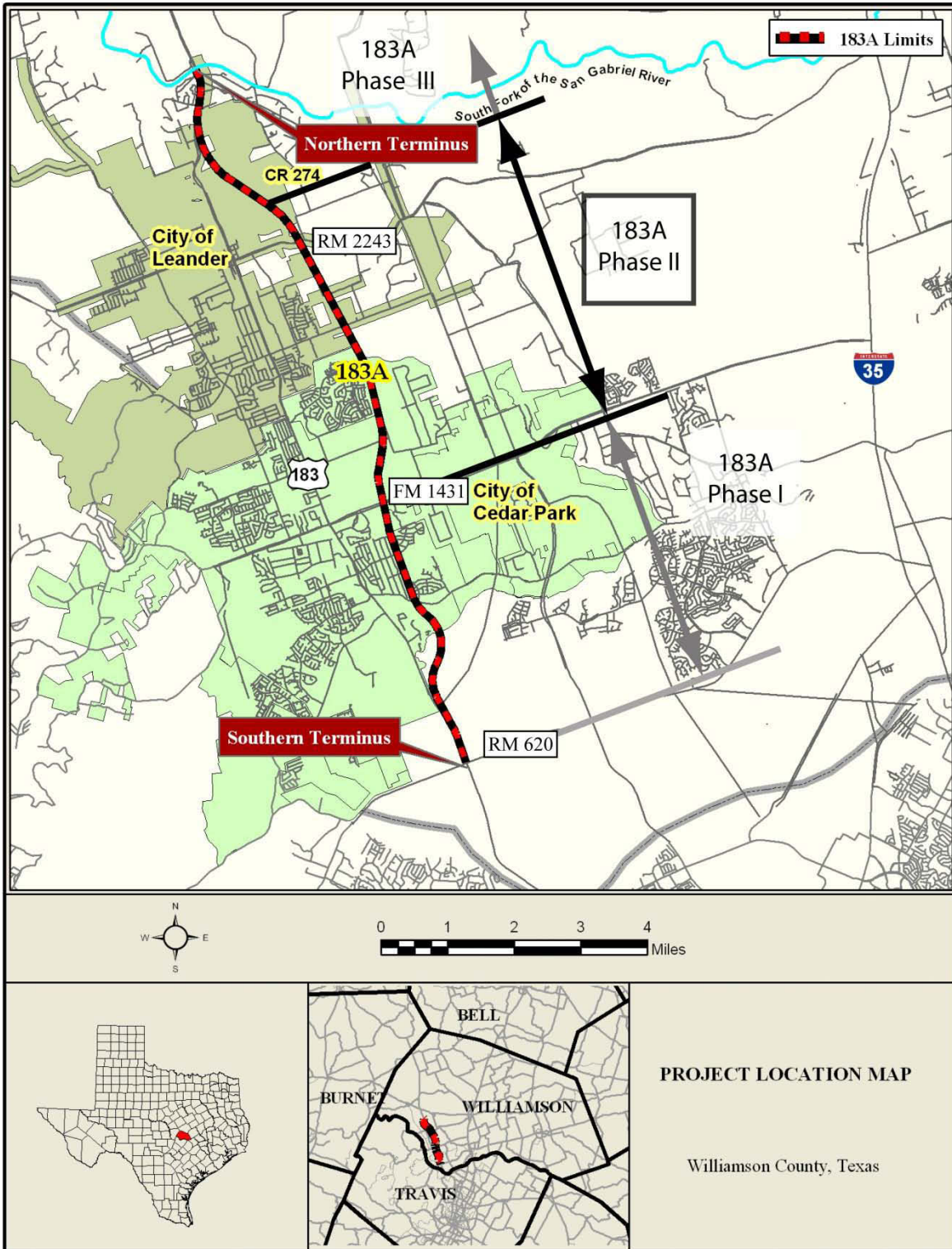


Figure 1:
Project Location

CONSTRUCTION ACTIVITIES

With over a third of the work finished, the construction of the 183A Phase II Project continues on its two-year timeline to complete approximately 5.1 miles of tolled mainlanes, 26 bridges, and the necessary toll collection systems.

On November 24, 2010, the Mobility Authority issued NTP 3 which concerns the construction of the 800 feet segment of San Gabriel Parkway (CR 274) outside and to the west of the 183A right of way.

During this reporting period, the first mainlane bridge beams were placed at the bridges over New Hope Drive and Block House Creek; this is a significant milestone for the construction of the 183A Phase II Project. Beams provide the support for the road surface on a bridge. The beams are brought in from Texas Concrete, a concrete plant in Victoria, Texas, approximately five counties away. For this project, the average bridge beam is 112 feet long and typically weighs over 96,000 lbs. The process of setting beams will continue at the multiple bridge locations along the project.

There is continuous excavation and grinding of rock at Scottsdale Drive. 183A Phase II mainlanes will be depressed in this section with a small bridge over the mainlanes to connect the existing frontage roads. In December, the team began the soil nail wall installation along the Scottsdale Drive cut areas for future retaining walls. Soil nailing is a stabilization method of reinforcing existing soil by installing steel bars into slopes or cuts.

A significant part of the current construction, and a critical path activity, is the fabrication and placement of MSE wall panels at all of the overpasses. This process is ongoing at multiple locations along the project.

Additionally, during the reporting period, the drilling activities for bridge substructures were substantially completed at locations along the project, including the proposed Hero Way and RM 2243. There will be additional drilled shaft construction at Scottsdale Drive, when excavation is completed.

The following tasks continue from the last reporting period: ongoing excavation of material and embankment building; construction of drainage facilities along the project; placement of columns and concrete caps at future bridge locations; ongoing traffic control activities; and erosion control.

The Mobility Authority's GEC continues to perform construction inspection and oversight of the Contractor, including all materials testing. Offsite material fabrication plant inspections continue to take place.

Mainlane Bridge Beam Placement at New Hope Drive and Block House Creek

Over the past three months, bridge beams have been placed at the two southernmost bridges – the overpasses over New Hope Drive and Block House Creek.



Beam Placement at New Hope Drive in October



New Hope Drive in December



Progress at Block House Creek. *Left:* October preparations for beam placement, *Right:* November beams

Moving North

The next set of bridges is at Crystal Falls Parkway and RM 2243. Progress continues in these areas.



Progress at Crystal Falls Parkway. *Left:* Columns in October – facing south, *Right:* Cap placement on columns in anticipation of beams in December – facing south



Progress at RM 2243. *Left:* Drilled shaft construction in October – facing north, *Right:* Column placement and the growing embankment in December – facing north

MSE Wall Panels

Both a critical path item and a significant part of the total construction, the placement of MSE wall panels and structures is ongoing along the project.



Placing MSE wall and structures south of RM 2243



Placing MSE walls and structures north of New Hope Drive



Placing MSE wall and structures south of Crystal Falls Parkway

Soil Nail Wall Installation

Another method of stabilization for retaining walls is the soil nail procedure. This is utilized in the Scottsdale Drive cut location.



Soil nail wall installation at Scottsdale Drive

PROJECT PROGRESS

Based on the assessment of the Contractor's activities and progress, the summary of the construction progress achieved on work tasks through the end of December 2010 is as follows:

**Table 1:
183A Phase II Construction Progress for Period Ending December 2010**

Construction Tasks	% Complete
Earthwork / Excavation / Embankment	75%
Stormwater Protection	65%
Drainage Structures	60%
Bridge Substructures	70%
Bridge Superstructure	20%
Retaining Walls	30%
Pavement Base	10%
Concrete Paving	0%
Asphalt Paving	15%
Toll Structures	0%
Electrical/Lighting / Signing / Signals	15%
Landscaping	0%

The assessment of the Contractor’s progress against the acceptable Baseline CPM (Critical Path Method) schedule for the reporting period indicates that the Contractor is maintaining the schedule and forecasting on-time substantial completion on March 23, 2012. There are no identified threats to the schedule at this time. **The Project is currently on schedule.**

As of December 31, 2010, thirty-nine (39%) of the 730 calendar days to substantial completion have expired and **construction is reported at 36% complete.**

**Table 2:
Schedule of Project Milestones**

Task	Date
Selection of Contractor	December 17, 2009
Early NTP	January 22, 2010
NTP 1 and NTP 2 Issued	March 24, 2010
Alternate Bid NTP Issued	May 7, 2010
NTP 3 Issued	November 24, 2010
Scheduled Substantial Completion (Open to Traffic)	March 23, 2012
Scheduled Final Completion	June 23, 2012

PROJECT FINANCIAL STATUS

The following summarizes the financial status of the Project through December 31, 2010.

Original Webber Contract Amount:	\$ 75,792,413.92
<i>Authorized Changes (Change Order and/or Amendments):</i>	
Change Order No. 01 (June 2010)	(\$551,363.58)
Change Order No. 02 (August 2010)	\$3,038.55
Change Order No. 03 (August 2010)	\$99,480.24
Change Order No. 04 (September 2010)	\$0
Change Order No. 05 (September 2010)	\$164,059.91
Change Order No. 06 (December 2010)	\$32,685.83
Change Order No. 07 (December 2010)	<u>(\$154,991.69)</u>
Current Authorized Contract Amount:	\$ 75,385,323.18

Webber Payments:	
Amount of Draw No. 01 (May 2010)	\$5,585,036.14
Amount of Draw No. 02 (June 2010)	\$4,331,734.34
Amount of Draw No. 03 (July 2010)	\$2,095,584.07
Amount of Draw No. 04 (August 2010)	\$2,236,166.04
Amount of Draw No. 05 (September 2010)	\$2,713,714.15
Amount of Draw No. 06 (October 2010)	\$3,182,080.23
Amount of Draw No. 07 (November 2010)	\$4,723,124.42
Amount of Draw No. 08 (December 2010)	<u>\$2,383,682.67</u>
Total Requested Amount To-Date through Draw No. 08:	\$ 27,251,122.06
Retainage withheld*:	<u>\$ 0.00</u>
Approved Amount for Work Completed through Draw No. 08:	\$ 27,251,122.06

Total Project Budget Expended Through December 2010: 36%

Amount remaining for work to be completed: \$48,134,201.12

*Retainage to be withheld only after 95% of the adjusted contract price has been paid.

Summary of Change Orders During Reporting Period

Change Order No. 6 incorporates costs for: additional embankment and excavation to correct unstable material at specific locations on the project; the furnishing, installation, and maintenance of ten (10) speed limit advisory signs for increased traffic safety throughout the project work zone; and traffic striping completed at the Cedar Park Center turnaround. The change order, a total of \$32,685.83, was submitted to the GEC on November 19, 2010; it was fully executed on December 3, 2010.

Change Order No. 7 incorporates cost credits for a change in the specification for asphalt placement on this project. This change is based the "Green Construction" initiative, which promotes sustainability and environmental responsibility. The initiative chiefly included the replacement of using warm mix asphalt, which allows a reduction in the temperature at which asphalt mixtures are produced and placed. These reductions have the benefit of cutting fuel consumption and decreasing the production of greenhouse gases. The change order, which credits the Mobility Authority \$154,991.69, was submitted to the GEC on December 9, 2010; it was fully executed on December 14, 2010.

Project Cash Flow Curve – Baseline

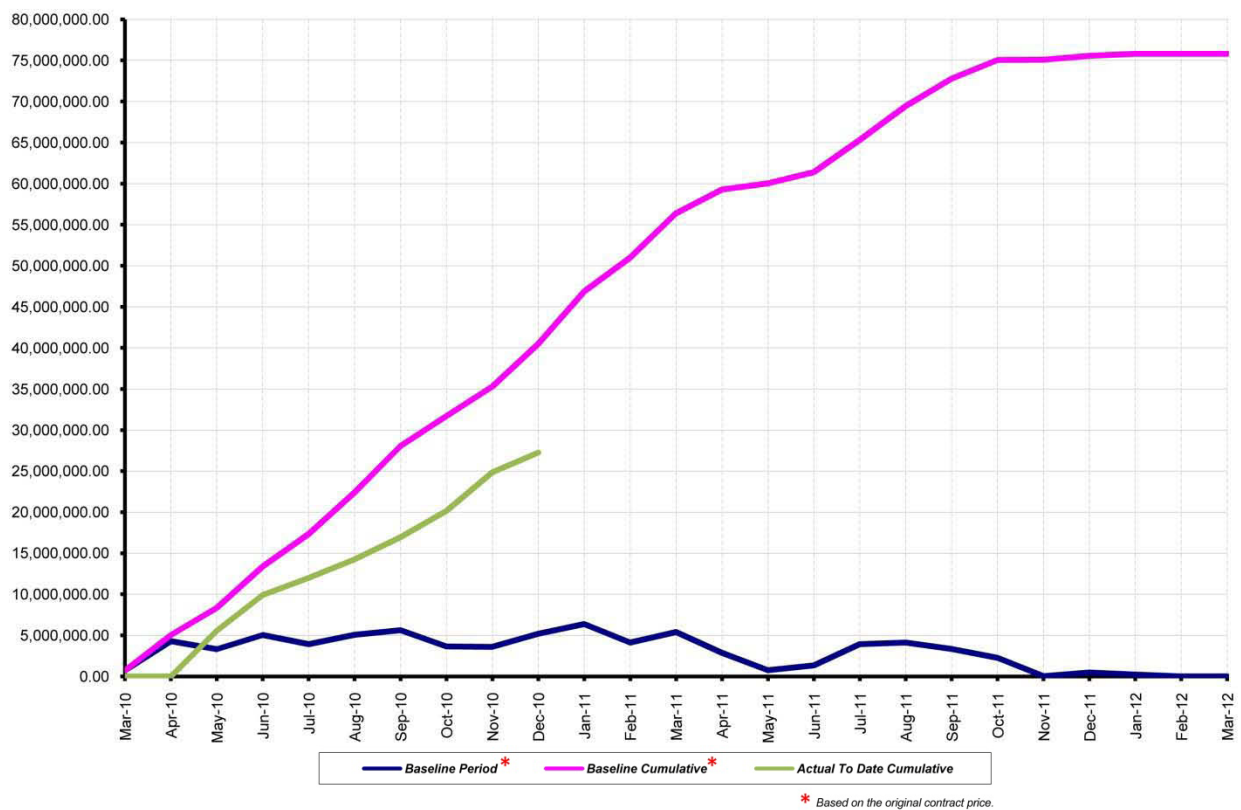


Figure 2:
Baseline Cash Flow Curve for Period Ending December 2010

DBE STATUS

Webber is required to meet the Disadvantaged Business Enterprise (DBE) goal of 11.62% for the project. The total DBE amount subcontracted to date is \$10,539,157 which is 13.98% of the authorized contract total. This represents executed DBE subcontracts with the following firms: Royal Vista, Indus, N-Line, Roadway Specialties, and TrevCon. To date, the DBE firms have been paid a total of \$3,552,529, which is 40.6% of the goal.

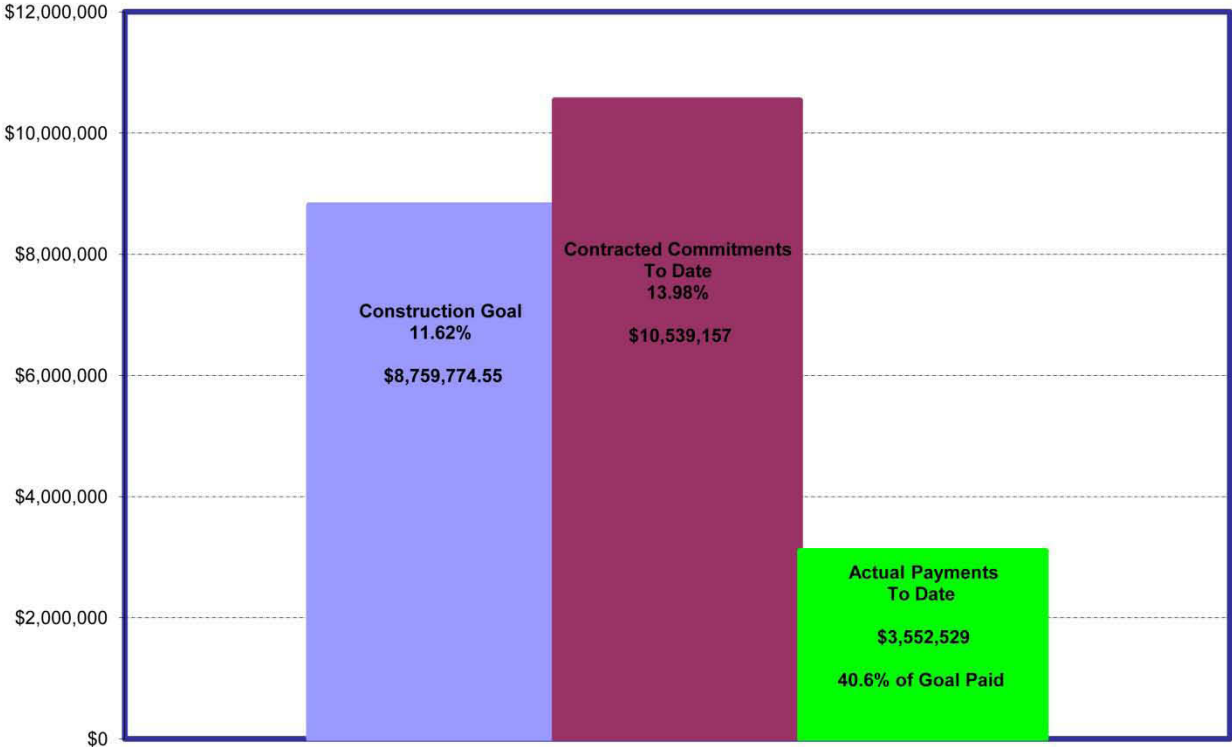


Figure 3:
DBE Construction Commitment vs. Payment
for Period Ending December 2010

PUBLIC INVOLVEMENT

The Mobility Authority's Public Involvement Team manages the 183A hotline (512-684-3256) and the project website (183A.com). Lane closures and construction alerts are regularly posted on the project website as well as posted on the project Twitter account (@183AExtension). Additionally, stakeholders can sign up on the project website for lane closure information to be sent directly to their cell phone via SMS text.

During the reporting period, several calls were made to the Mobility Authority Public Involvement Team about the construction, particularly regarding concerns over noise and dust. In each case, the resident received a personal call or email from the team and was informed how the issue was being addressed. The Team follows up on these types of calls regularly.



On November 13, the 183A Construction team staffed a Community Appreciation Event for Block House Creek and Ridgewood South, two neighborhoods directly adjacent to the ongoing construction. Attended by over 500 local residents, the event featured an appearance from the American Hockey League's Texas Stars players and Ice Girls, who signed autographs and gave out incredible raffle prizes. Food and drink were provided as well as a live band, multiple bouncy castles, a balloon artist, face painting, cotton candy, and popcorn.

Construction equipment was available for photo ops. The Mobility Authority and Webber sponsored the event.



Community Appreciation Event

Upper left: Lunchtime for the attendees; *lower left:* Party in the park with the family and neighbors; *lower right:* Autographs and big smiles from local Texas Stars players